



Board of Zoning Appeals
January 11, 2024 - 6:00 PM
233 South Main Street, Monroe, Ohio

Call to Order

- Roll Call

Election of Officers

Approval of Minutes

- July 10, 2023 Meeting Minutes

Old Business

New Business

- *Public Hearing*
Consideration of a variance application from Carra Builders regarding two fuel canopies for a proposed Marathon Gas Station.

Adjournment



**Board of Zoning Appeals
Minutes July 10, 2023 –
6:00pm**

The Board of Zoning Appeals of the City of Monroe met in regular session at 6:00 pm on July 10, 2023. The meeting was held in the Council Chambers located at 233 South Main Street.

Call to Order

Mr. Adam called the meeting to order at 6:01 p.m.

Roll Call

Members present: Lorenzo Adam, Joshua Mason, Mike Morris, Matt Romito.

Members absent: Casey Walters

Staff members present: Tom Smith, Director of Development, and Jameson Cole, Planner

Mr. Morris moved to excuse Mr. Walters for the meeting. Seconded by Mr. Romito. Voice vote. Motion carried.

Approval of Minutes

Mr. Adam requested a motion to approve the minutes from the May 15, 2023, Board of Zoning Appeals meetings.

Mr. Morris moved to approve the minutes from the May 15, 2023, Board of Zoning Appeals meeting. Seconded by Mr. Romito.

Voice vote; motion carried.

Old Business

None.

New Business

Public Hearing: Case No. 2023-2- Consideration of a variance application by applicant Terrence Frank regarding an expansion to a legally non-conforming use



Having been sworn, Mr. Cole gave the following presentation:

Background:

- Terence Frank, on behalf of Corken Steel, located at 341 Brearden Dr, has applied for variance regarding a proposed warehouse expansion to a legally nonconforming use.
- The applicant wishes to expand their existing warehouse.
- The applicant proposes to construct a 12,000 SF addition for the warehousing of equipment and products.
- The property owned by Corken Steel is 1.92 acres and is zoned C-1 Highway Commercial

Variance Purpose:

- To allow the expansion of a Nonconforming Use.
- Section 1207.05(E)(2) outlines this requirement as the following:

Variance Request:

- To allow the use of galvanized steel for the chain link fence in place of the plastic black coating and placing barb wire on top of the fence.
- Section 1204.05(A)(e) outlines this requirement as the following:

Notwithstanding the foregoing provisions to the contrary, a structure containing a nonconforming, nonresidential use, may be increased or improved, regardless of the applicable zoning district, where the owner of such use can demonstrate through application to the BZA that the manner in which the useable area of the nonconforming use will be increased or improved will have minimal adverse impact upon adjacent properties and other permitted land uses in the surrounding neighborhood or can be made compatible with the adjacent properties and the uses in the surrounding neighborhood upon compliance with specified conditions.

Review Criteria:

In order to grant a use variance, the BZA shall determine that strict compliance with the terms of this code will result in unnecessary hardship to the applicant. The applicant must demonstrate such hardship by clear and convincing evidence that all of the following criteria are satisfied. Staff's findings are listed after each criterion:

The property cannot be put to any economically viable use under any of the permitted uses in the zoning district in which the property is located;



There are a limited number of permitted uses that would be able to utilize the structure as it is currently situated on the property. In general, the property could be put to economically viable use for most, if not all, permitted uses within C-1 Highway commercial.

The variance requested stems from a condition which is unique to the property at issue and not ordinarily found in the same zoning district;

The variance request stems from creation of a new zoning map during the 2010-2012 zoning code update. This property is unique to other properties adjacent to and located within the same zoning district because it was one of the few properties during this time to have their zoning designations changed creating the nonconformity.

The variance requested cannot otherwise be resolved by a zoning map amendment;

The variance requested could be resolved by a zoning map amendment.

The zoning map is currently being updated through the City's zoning code update. This property could be rezoned to L-1 Light industrial as it was prior to becoming C-1 Highway commercial.

That there is an existing structure that cannot be reasonably used for a use permitted within the applicable zoning district;

The current existing structure is being utilized by Corken Steel but because of demand they are in need of additional space for their production, storage, and sales of their product.

The hardship condition is not created by actions of the applicant;

During the 2010-2012 zoning code update, the zoning map changed and the parcel in question was rezoned from L-1 Light Industrial to C-1 Highway Commercial.

Corken Steel received their new business permit January 30, 2009.

The granting of the variance will not adversely affect the rights of adjacent property owners or residents;



Granting the variance will not adversely affect the rights of the adjacent property owners because the request is not changing or impacting the use of the site. The only change resulting from this variance is an addition to the current building, which will be brought up to code through parking, landscaping, and stormwater. Code compliance with these items will ensure no property owner's rights are adversely affected.

The granting of the variance will not adversely affect the public health, safety or general welfare;

Granting of this variance will not adversely affect the public health, safety or general welfare because the addition is in the rear of the property, not visible from the road, and will be used for the same purposes as the primary structure.

The variance will be consistent with the general spirit and intent of this code;

The spirit and intent behind nonconforming uses reads as follows:

“Nevertheless, while it is the intent of this code that such nonconformities be allowed to continue until removed, they should not be encouraged to survive, unless otherwise allowed in this chapter or specifically addressed in this code.”

The variance is consistent with the spirit and intent of this code because they are seeking permission from the board to do an expansion of their use which is nonconforming. Through this variance they are to demonstrate that this improvement will have minimal adverse impact upon adjacent properties and other permitted uses.

The variance sought is the minimum that will afford relief to the applicant.

The granting of this variance would provide this applicant the ability to expand their nonconforming use, which is generally not permitted. With the zoning code update changing the zoning map at the end of the year, a zoning map amendment would not have solved the problem. With other nonconforming uses surrounding the site, this would set a precedent for other nonconforming uses to expand in the future.

Department Comments:

- Public Works/Engineering: No comment.
- Police Department: No comment.
- Fire Department: No comment.

Public Comments:

- None.

Staff Recommendation:

- As a reminder, a variance from the requirements of this Code may be granted by the Board of Zoning Appeals when the Board determines that such a variance will not be contrary to the public interest and that, due to special conditions, practical difficulty or unnecessary hardship exist that prevent strict application of this Code.
- Staff's review and recommendation are based on Code interpretations as well as the potential for precedents that could affect overall enforcement of City standards and regulations. The BZA's approval, denial, or modification is not required to be based on all the above criteria being met. No single factor listed above may control and not all factors may be applicable in each case. Each case shall be determined on its own facts.

Staff does not make a recommendation regarding a variance request. The expansion of a legally nonconforming use is permitted by code section 1207.05 (E)(2). The applicant's need for expansion (production, storage, sales) are the result of a profitable business. Use variances are rare, and expanding nonconforming uses are less common. The property is unique because the use was permitted and appropriately zoned prior to the 2010-2012 code update. This property was the only property to have its zoning change when compared to the adjacent properties. The request is considerable as the code reads and is not the minimum necessary as they could go for a zoning map amendment. As mentioned in the report, conducting a zoning map amendment now could be ineffective as the zoning code and map are being updated.

Mr. Cole submitted the documents for the record.

Mr. Adam asked what made this property nonconforming. Mr. Cole stated the map changed and it was converted at that time. Mr. Adam asked if this is the only nonconforming property in this area. Mr. Cole stated he believed there is more than just this property.

Mr. Morris asked what the overlay is that is being proposed for the code update. Mr. Cole stated no maps have been created. Mr. Smith stated the City is proposing that this area be changed to light industrial, L1.

Steve Schmidland, with Unit Building Service representing Corkin stated he was surprised this was a nonconforming use. Mr. Schmidland stated he believes this will be an attractive addition.

Mr. Morris asked if this would create any new jobs. A member of the audience stated that yes, it is possible.



Mr. Morris moved to close the public hearing; seconded by Mr. Romito. Voice vote. Motion carried.

Mr. Mason moved to accept the documents; seconded by Mr. Romito. Voice vote. Motion carried.

Mr. Romito stated he believes this property should be rezoned to light industrial (referencing the current code update)

Mr. Morris motioned to approve Case No. 2023-2 Consideration of a variance application by applicant Terence Frank regarding an expansion to a legally non-conforming use. Seconded by Mr. Romito.

Roll call vote. Motion carried.

Mr. Adam moved to adjourn meeting; seconded by Mr. Morris. Voice vote. Motion carried.

The Board of Zoning Appeals meeting adjourned at 6:22 p.m.

Respectfully submitted by:

Michelle Layman

Planning and Zoning Specialist



CITY OF MONROE
Inter-Office Correspondence

TO: Members of the Monroe Board of Zoning Appeals
FROM: Kameryn Jones
Assistant Development Director

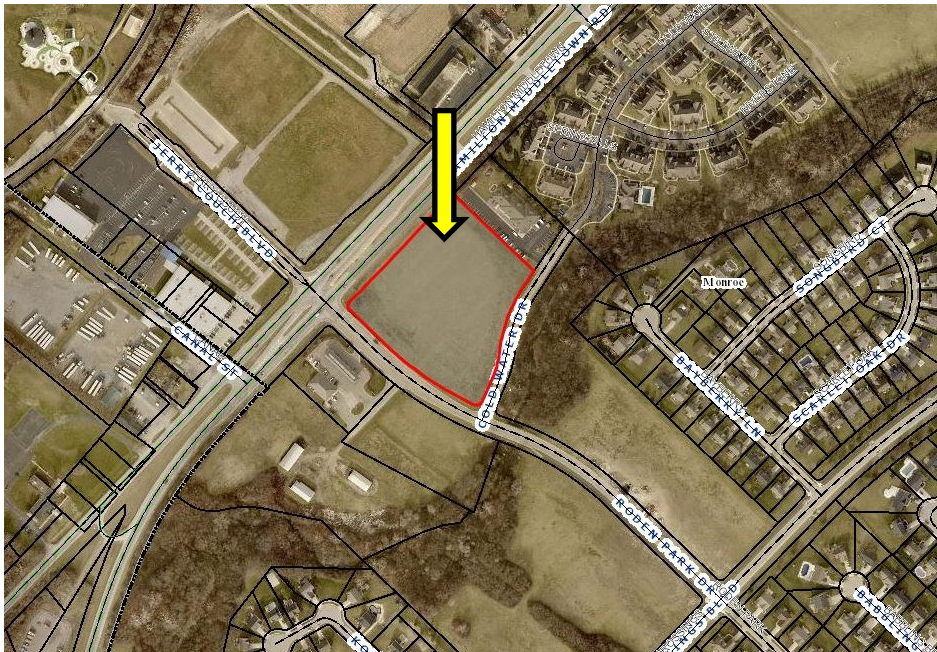
DATE: January 11, 2024

RE: Information Packet for **January 11, 2024** Board of Zoning Appeals Meeting at **6:00 p.m.**

I. BZA-2024-1— Consideration of a variance application from Carra Builders regarding two fuel canopies for a proposed Marathon Gas Station

A. Background:

- Jason Caldwell of Carra Builders, on behalf of Shops of Monroe Crossings LLC, has applied for a variance regarding fuel canopies at a proposed convenience store and retail fuel sales use at the corner of Roden Park Drive and Cold Water Drive.
- Carra Builders requests to exceed the maximum clearance height of 14 feet permitted for fuel canopies for both the standard and diesel fuel canopies proposed to be located on the south and east sides of the site.
- The applicant seeks to construct a standard refuel area with a 15-foot clearance height and a diesel refuel area with a 16-foot clearance height to accommodate semi-trucks, which the applicant states are 14 FT in height.
- The proposed site is 5.35 acres and is zoned C-4 Gateway Commercial PUD.



Overhead View of Site



Surrounding Zoning (the yellow arrow represents the proposed project's site, and the dark blue represents the section of the PUD that went through a zone change to R-4 Multi-Family Residential PUD.)



Facing east towards the site from Hamilton Middletown Road



Facing northeast towards the site from the corner of Roden Park Dr and Hamilton Middletown Road



Facing northwest towards the site from the corner of Roden Park and Cold Water Drive

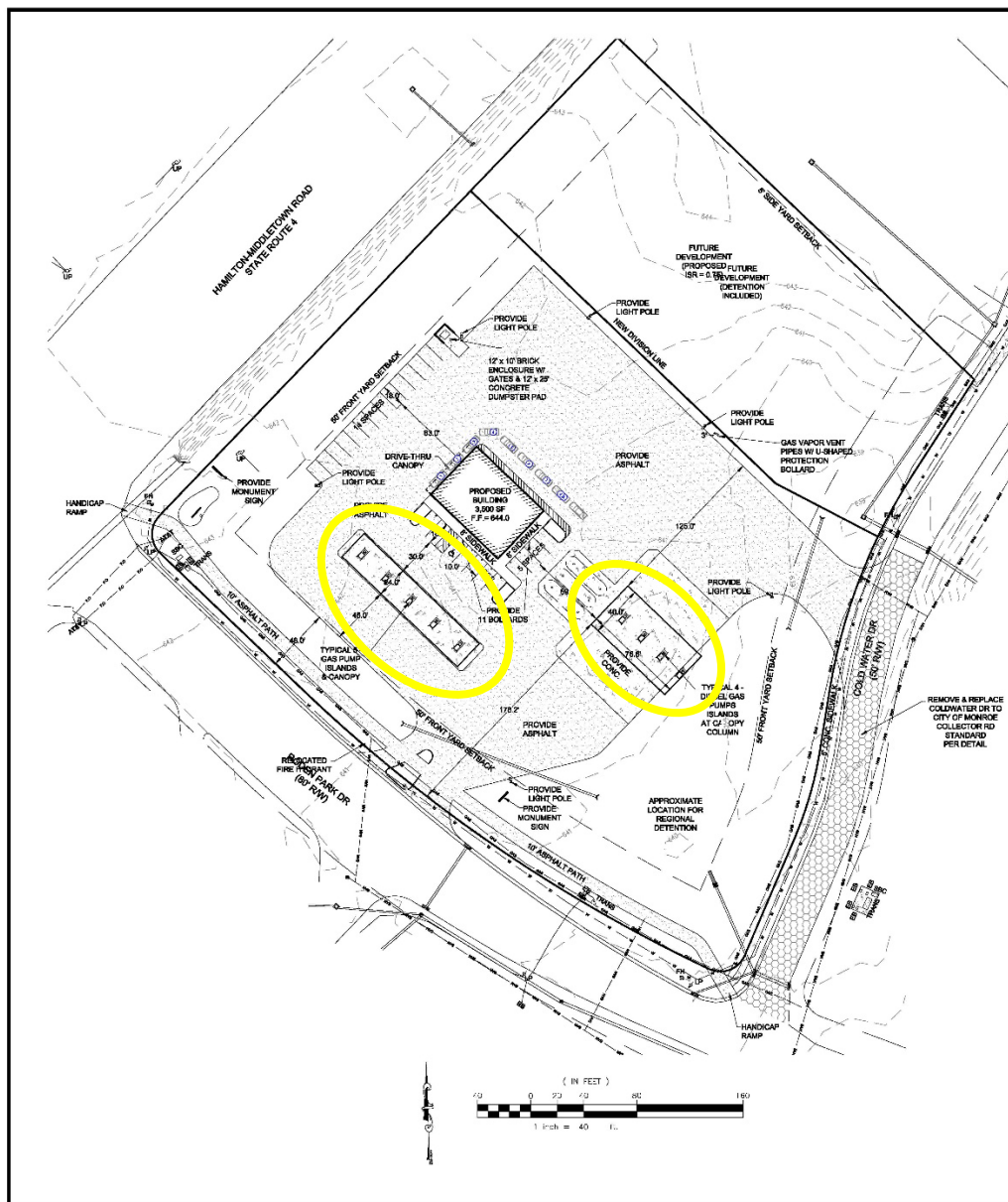
B. Variance Purpose:

- A variance from the requirements of this code may be granted by the BZA when the BZA determines that such a variance will not be contrary to the public interest and that, due to special conditions, practical difficulty or unnecessary hardship exist that prevent strict application of this code.

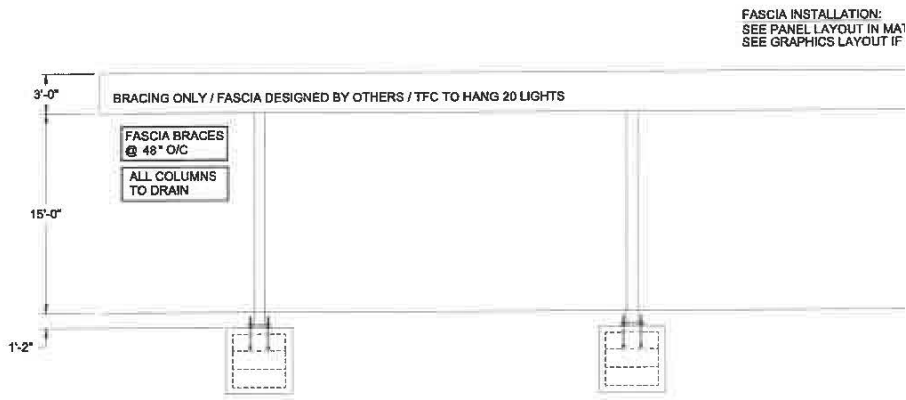
C. Variance Request:

- To allow an increase in the maximum clearance height permitted for fuel canopy structures from 14 feet to 15 feet and 16 feet.
- Section 1204.04(D)(4) outlines this requirement as the following:

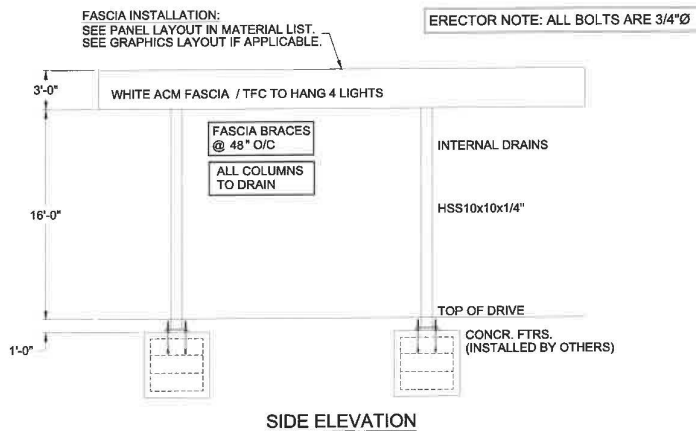
a) Canopies shall be gabled, set back a minimum of 15 feet from street rights-of-way, and shall not exceed a clearance height of 14 feet.



Proposed Site Plan



Proposed Standard Fuel Canopy Elevation



Proposed Diesel Fuel Canopy Elevation

D. Review Criteria:

The following factors shall be considered and weighed by BZA to determine practical difficulty. Staff's findings are listed after each criterion:

- ***Whether special conditions and circumstances exist which are peculiar to the land or structure involved and which are not applicable generally to other lands or structures in the same zoning district; examples of such special conditions or circumstances are: exceptional irregularity, narrowness, shallowness or steepness of the lot, or adjacency to nonconforming and inharmonious uses, structures or conditions.***

The only special condition of this site that differs from similar commercial properties within the same zoning district is that it has three front yards.

- ***Whether the property in question will yield a reasonable return or whether there can be any beneficial use of the property without the variance;***

The property may yield a reasonable return and be of beneficial use without the requested variance as a convenience store and retail fuel sales or any other use under the C-4 zoning district.

- ***Whether the variance is substantial and is the minimum necessary to make possible the reasonable use of the land or structures;***

The variance requested is substantial as the request is only to increase the clearance height to allow for semi-truck diesel refueling. It is the interpretation of Staff that it is not necessary to make use of the land.

- ***Whether the essential character of the neighborhood would be substantially altered or whether adjoining properties would suffer substantial detriment as a result of the variance.***

This site is different from other convenience store and retail fuel sales that offer semi-truck refueling because of its proximity to an approved residential PUD and the residential subdivision of Monroe Crossings. As described in public feedback received, the proposed use is also adjacent to a daycare facility, which is described as a cause for concern for safety and noise from larger vehicles accessing and leaving the site.

- ***Whether the variance would adversely affect the delivery of governmental services such as water, sewer, refuse pickup;***

The variance would not adversely affect the delivery of governmental services.

- ***Whether special conditions or circumstances exist as a result of actions of the owner;***

The requested variance is due to the stated need of the business owner to raise the clearance level of the refueling canopies to allow for larger vehicles.

In their application, the applicant states the business model for fueling sales is hindered by clearance requirements for both canopies.

- ***Whether the property owner's predicament can feasibly be removed through some method other than a variance;***

The applicant could construct the convenience store and retail fuel sales while staying under the maximum fuel canopy clearance height of 14 FT.

The applicant has stated that the height of the canopies hinders their business model for fueling sales.

- ***Whether the spirit and intent behind the code requirement would be observed and substantial justice done by granting a variance;***

The requested variance may not meet the spirit and intent behind the code.

- ***Whether the granting of the variance requested will confer on the applicant any special privilege that is denied by this regulation to other lands, structures, or buildings in the same district;***

The granting of this variance would provide this applicant with the ability to construct the fuel canopies over the maximum height allowed for other commercial properties in the same C-4 zoning district. Staff would note that the convenience store and retail fuel sales across the street are

operating within the same C-4 Gateway Commercial PUD zoning district with a compliant fuel canopy.

E. Department Comments:

- Public Works/Engineering: No comment.
- Police Department: No comment.
- Fire Department: No comment.

F. Public Comment:

- Staff have received numerous letters from the public. These letters have been included in the packet.

G. Conclusion:

- As a reminder, the applicant shall be required to supply evidence that demonstrates that the literal enforcement of this code will result in practical difficulty for an area/dimensional variance.
- Staff's review is based on Code interpretations as well as the potential for precedents that could affect overall enforcement of City standards and regulations. The BZA's approval, denial, or modification is not required to be based on all the above criteria being met. No single factor listed above may control and not all factors may be applicable in each case. Each case shall be determined on its own facts.



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TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-6665 FAX (260) 357-6533

TABLE OF CONTENTS	
DRAWING TITLE	DRAWING # OF 3
PLAN VIEW & ELEVATIONS	1
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CODE SPECIFICATIONS

I.C.C. INTERNATIONAL BUILDING CODE (2015 WITH A3-21 OHIO AMENDMENTS)
ASCE/SEI 7-10
AISC STEEL CONSTRUCTION MANUAL, FIFTEENTH EDITION
AISC CODE OF STANDARD PRACTICE FOR BUILDINGS AND BRIGES (2010 EDITION)
AISI SPECIFICATIONS FOR THE DESIGN OF COLD-FORMED STEEL STRUCTURAL MEMBERS (S100-2015)
AWS D1.1-2011 STRUCTURAL WELDING CODE
INTERNATIONAL PLUMBING CODE, 2016
AISC SPECIFICATION FOR THE DESIGN, FABRICATION AND ERECTION OF STRUCTURAL STEEL FOR BUILDINGS (335-8851)

2017 OHIO BUILDING CODE

1603.1.1 FLOOR LIVE LOAD: N/A
1603.1.2 ROOF LIVE LOAD: 20 PSF
1603.1.3 ROOF SNOW LOAD:
Pg = 20 PSF
Ps = 20.00 PSF
Cs = 1.0
Is = 1.0
Cs + WsA
Ps1s = 22.58 PSF
Ps1s = 17.08 PSF
Ws = 5.44 FT
Ws = 4.12 FT

1603.1.4 WIND LOAD:
V30 SEC (UST) = 115 MPH
Wind = 90 MPH
I = 1.0
EXPOSURE = C
GC2 = 0.80
RISK CATEGORY = II

1603.1.5 EARTHQUAKE DESIGN DATA
RISK CATEGORY = II
Is = 1.0
Ss = 0.143
SI = 0.074
SITE CLASS = D
Sds = 0.153
SFI = 0.118
SEISMIC DESIGN CATEGORY = B
SEISMIC RESISTING SYSTEM =
CANTILEVERED COLUMN + GC
BASE SHEAR = 0.37 MW/COLUMN
Cs = 0.122
CONSTRUCTION TYPE: TYPE IIB
USE GROUP: MERCANTILE-GROUP M
R = 1.25
ANALYSIS PROCEDURE =
EQUVALENT LATERAL FORCE

ERECTOR NOTES	BOLT NOTES:
A. ALL LEVELING NUTS AND FOOTING ELEVATIONS MUST BE CHECKED WITH A TRANSIT. VERIFY ALL FOOTING BOLT CENTERS.	ALL A325 AND A490 BOLTED CONNECTIONS MARKED WITH THE DESIGNATION (ST) SHALL BE INSTALLED AS SNUG-TIGHTENED JOINTS AS DEFINED IN SECTION 8.1 OF THE RCSC SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS, DECEMBER 31, 2006, CONTAINED IN PART 16, SPECIFICATIONS AND CODES OF THE AISC STEEL CONSTRUCTION MANUAL, FOURTEENTH EDITION.
B. CHECK ALL STEEL FOR PLUMB, SQUARE, AND LEVEL AFTER IT IS ERECTED.	8.1 SNUG-TIGHTENED JOINTS "... THE SNUG-TIGHTENED CONDITION IS THE TIGHTNESS THAT IS ATTAINED WITH A FEW IMPACTS OF AN IMPACT WRENCH OR THE FULL EFFORT OF AN IRONWORKER USING AN ORDINARY SPUD WRENCH TO BRING THE CONNECTED PLIES INTO FIRM CONTACT."
C. ALL ASTM A325 AND A490 BOLTS SHALL BE INSTALLED PER THE RCSC SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS, (JUNE 30, 2004) CONTAINED IN PART 16, SPECIFICATIONS AND CODES OF THE AISC STEEL CONSTRUCTION MANUAL, 14TH EDITION.	INSPECTION REQUIREMENTS FOR SNUG-TIGHTENED JOINTS ARE AS LISTED IN SECTION 8.1 OF THE RCSC SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS, DECEMBER 31, 2006, CONTAINED IN PART 16, SPECIFICATIONS AND CODES OF THE AISC STEEL CONSTRUCTION MANUAL, FOURTEENTH EDITION.
D. CHECK LEVELNESS OF BOTTOM OF PURLINS WITH A TRANSIT.	9.1 SNUG-TIGHTENED JOINTS "... AFTER THE CONNECTIONS HAVE BEEN ASSEMBLED, IT SHALL BE VISUALLY ENSURED THAT THE PILES OF THE CONNECTED ELEMENTS HAVE BEEN BROUGHT INTO FIRM CONTACT AND THAT WASHERS HAVE BEEN USED. ... NO FURTHER EVIDENCE OF CONFORMITY IS REQUIRED FOR SNUG-TIGHTENED JOINTS. THE MAGNITUDE OF THE CLAMPING FORCE THAT EXISTS IN A SNUG-TIGHTENED JOINT IS NOT A CONSIDERATION."
E. ALL BRACING FOR BEAMS AND PURLINS IS TO BE WELDED ON BEFORE DECK PANELS ARE INSTALLED.	
F. FIELD PRIME ALL STRUCTURAL STEEL CONNECTIONS AND SPLICES AFTER STEEL IS ERECTED.	
G. WASHERS ARE REQUIRED ON ALL BOLTED CONNECTIONS. THE NUT AND WASHER SHALL BE PLACED AT THE TOP OF CONNECTION (NUT SIDE IF BOLT RUNS HORIZONTALLY). A WASHER IS REQUIRED FOR ALL BOLTED CONNECTIONS AT SLOT LOCATIONS (UNLESS NOTED OTHERWISE).	
H. DO NOT DEVIATE FROM PLANS WITHOUT PRIOR TFC CANOPY CONSENT.	
MATERIAL SPEC.	GENERAL NOTES
HOT-ROLLED STRUCTURAL - A882 STRUCTURAL TUBE - A500 GRADE B PLATE - A528 COLD-FORMED STRUCTURAL - A1011 (GRADE A (FY=66 KS)) DECK PANELS - A653 (FY=50 KSI) WELDS - AWS/D1.1 E70XX ELECTRODE HIGH STRENGTH BOLTS - A325N WASHERS - F436	I. DO NOT SCALE DRAWINGS II. ALL CONSTRUCTION SHALL CONFORM TO APPLICABLE STATE AND LOCAL CODES. III. ALL WELDING UNLESS SPECIFICALLY NOTED SHALL BE PERFORMED BY THE COMPANY SUPERVISOR. SHIPMENT, ALL WELDS SHALL BE FULL WELDS, A MINIMUM OF 1/4" SIZE AND PERFORMED BY CERTIFIED WELDERS, UNLESS NOTED OTHERWISE.

CERTIFIED BY:

02/09/2022

REV.	DESCRIPTION	DATE	BY

LOCATION: Roden Park C-Store (Arco-Diesel)
Monroe, Ohio

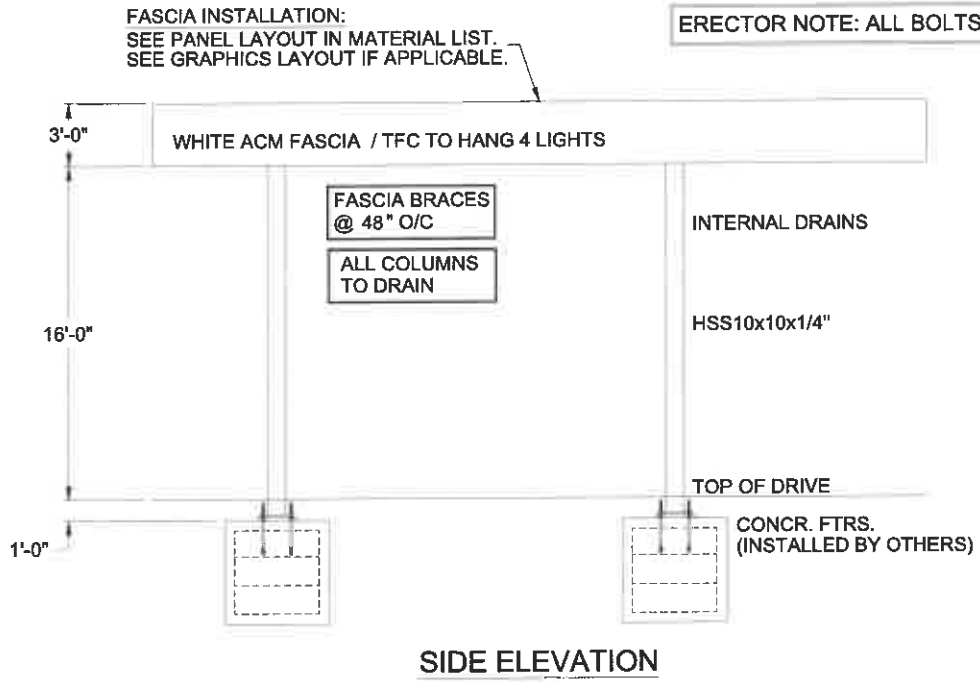
CUSTOMER: JF Petroleum Group
Dayton, OH

SCALE: DO NOT SCALE
ENGINEER: KED
DATE: Rev. 12/2023
DRAWN BY: BTD
DRAWING NO. COVER

ERECTOR CONSTR STEEL JOB FILE

JOB #
24'-0" x 72'-0" -- 6 Cols.

JOB REF. N/A

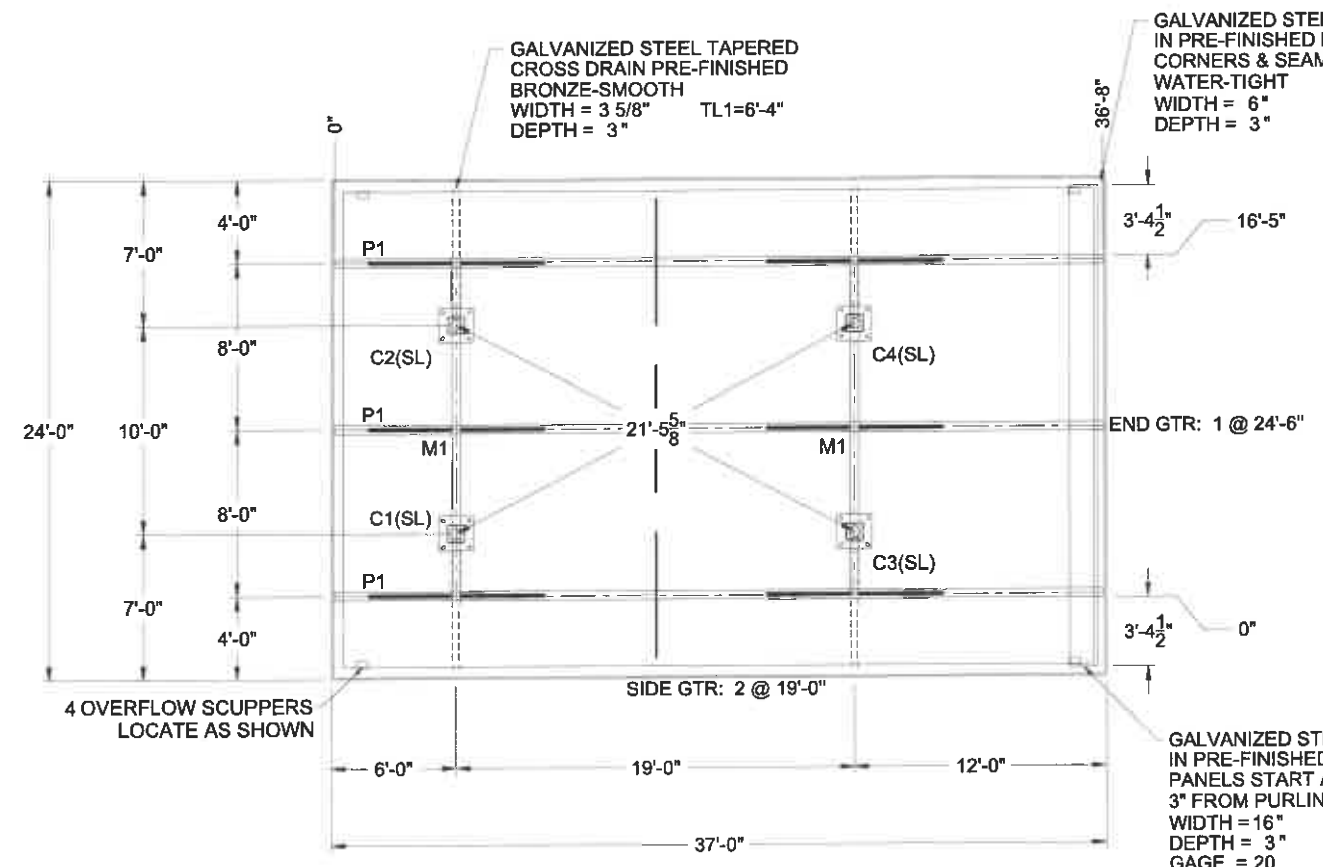
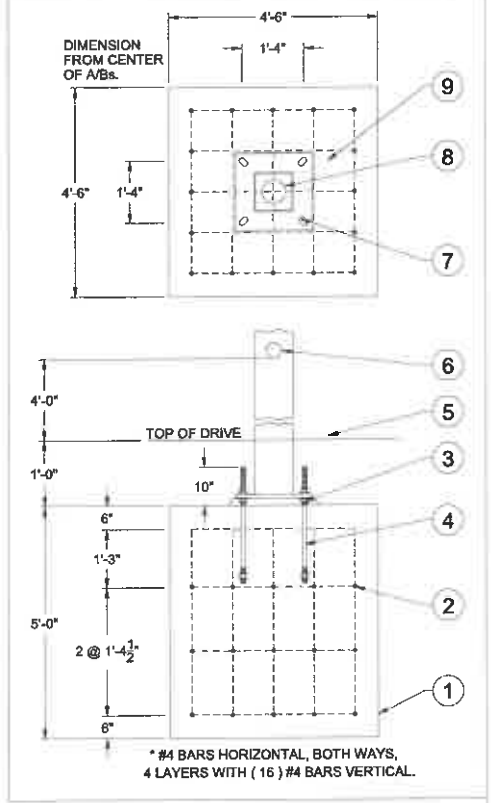


- 1'-2" PURLIN HEIGHT
- 1'-0" FOOTER SUBSET
- 16'-0" CLEARANCE HEIGHT
- + 0'- 6" DECK & GUTTER DEPTH
- 18'-8" FOOTER TO MAINBEAM HEIGHT
- 0'- 3" TOP & BOTTOM PLATE THICKNESS
- 18'-5" COLUMN CUT LENGTH

FOOTER NOTES

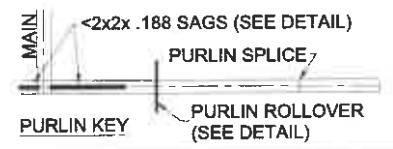
1. CONCRETE BASE BY GENERAL CONTRACTOR.
2. SEE FOOTER DETAIL FOR REQUIRED REBAR SPACING. REBAR Fy= 50 ksi.
3. BAREPLATE TO BEAR ON LEVELING NUTS AND WASHERS TO CREATE A MINIMUM 1 1/8" GROUT POCKET. GENERAL CONTRACTOR MUST FILL UNDER THE BASE PLATE WITH A NON-SHRINK GROUT THAT MEETS THE PERFORMANCE REQUIREMENTS OF ASTM C 1107 AND CRD-C 821 SPECIFICATIONS FOR NON-SHRINK GROUT. MIXING AND PLACING OF GROUT SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
4. 1 1/2" x 3/8" I.G. F1554 GRADE 55 ANCHOR BOLTS W/ HEAVY HEX NUT AND 10' THREADED PROJECTION WITH DOUBLE NUTS FOR PLUMBING AND LEVELING.
5. ALLOW A 3" SQUARE OPENING IN DRIVE AT COLUMNS FOR INSTALLATION OF ANCHOR BOLT NUTS. OPENING TO BE FILLED WITH CONCRETE AFTER ERECTION.
6. ELECTRICAL ACCESS OPENING (4"Ø HOLE) WITH 5/8"Ø COVER PLATE LOCATED APPROXIMATELY 4" ABOVE TOP OF DRIVE.
7. BASE PLATES ARE 20"x20" x 1" THK GR. 50 STL. W/ (4) - 1 1/2"Ø BOLT HOLES ON 16" x 16" CENTERS.
8. BASE PLATE HAS A 6"Ø HOLE FOR ELECTRICAL ACCESS.
9. UNLESS OTHERWISE NOTED, TOPS OF FOOTERS MUST BE HELD LEVEL AND ON THE SAME PLANE.
10. CONCRETE FOR FOOTERS SHALL ACHIEVE A MINIMUM COMPRESSION OF 4000 PSI.
11. FOOTING DESIGN IS BASED ON PRESUMPTIVE LOAD BEARING VALUES PROVIDED IN THE INTERNATIONAL BUILDING CODE, TABLE 1806.2. AN ASSUMED ALLOWABLE BEARING PRESSURE OF 2000 PSF AND A LATERAL BEARING PRESSURE OF 150 PSF/FT SHALL BE VERIFIED IN THE FIELD. IT IS RECOMMENDED THAT FOOTINGS BEAR ON STABLE ORIGINAL UNDISTURBED SUBGRADE OR SELECT CONTROLLED FILL (EXTENDING TO STABLE ORIGINAL UNDISTURBED SUBGRADE). IF FIELD CONDITIONS AND TESTING INDICATE LESS CAPACITY OR DISTURBED SOIL, THE DETAIL SHOWN SHALL NOT BE USED AND TFC SHALL BE CONTACTED IMMEDIATELY TO REDESIGN THE FOOTINGS.

FOOTER DETAILS



CANOPY PLAN VIEW

PURLINS ARE ("P" MARKS)
W14x22 ~U.N.O.
MAIN BEAMS ARE ("M" MARKS)
W14x22 ~U.N.O.
Canopy length for 3 pump 6 Column island 72'



TFC

TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-8665 FAX (260) 357-6533

LOCATION:	Roden Park C-Store (Arco-Diesel) Monroe, Ohio
CUSTOMER:	JF Petroleum Group Dayton, OH
SCALE:	DO NOT SCALE
DATE:	Rev. 12/2023
ENGINEER:	KED
DRAWN BY:	BTD
DRAWING NO.:	1 OF 3

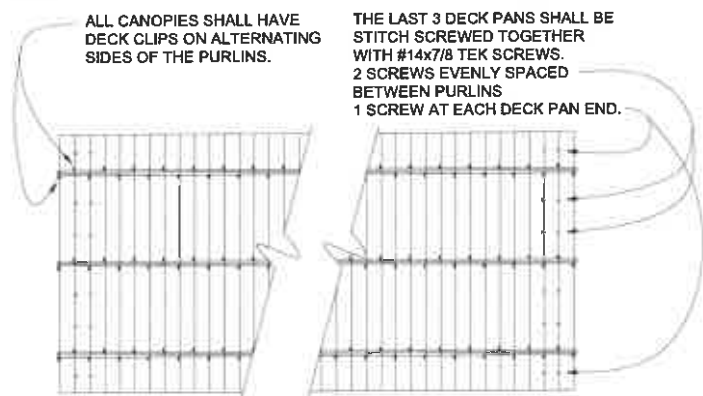
REV.	DESCRIPTION	DATE	BY

ERECTOR CONSTR STEEL JOB FILE

JOB #

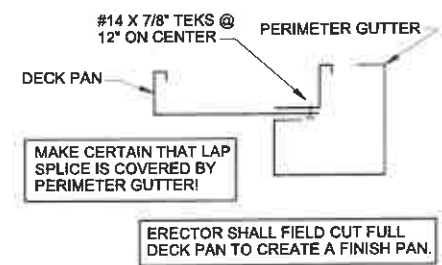
PLAN VIEW & ELEVATIONS

JOB REF. N/A



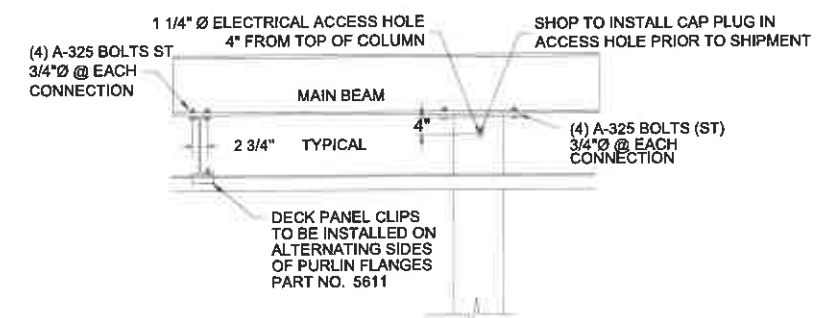
DECK ATTACHMENT DETAIL

ALL CANOPIES SHALL HAVE DECK CLIPS ON ALTERNATING SIDES OF THE PURLINS.
 THE LAST 3 DECK PANS SHALL BE STITCH SCREWED TOGETHER WITH #14x7/8 TEK SCREWS. 2 SCREWS EVENLY SPACED BETWEEN PURLINS. 1 SCREW AT EACH DECK PAN END.

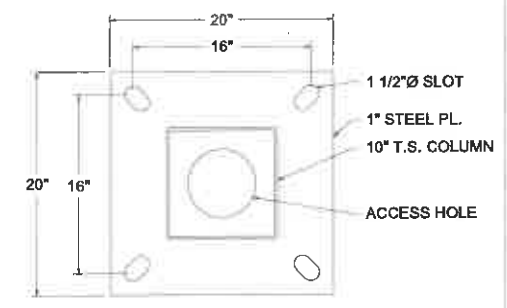


FINISH DECK PAN DETAIL

ERECTOR SHALL FIELD CUT FULL DECK PAN TO CREATE A FINISH PAN.
 MAKE CERTAIN THAT LAP SPLICE IS COVERED BY PERIMETER GUTTER!

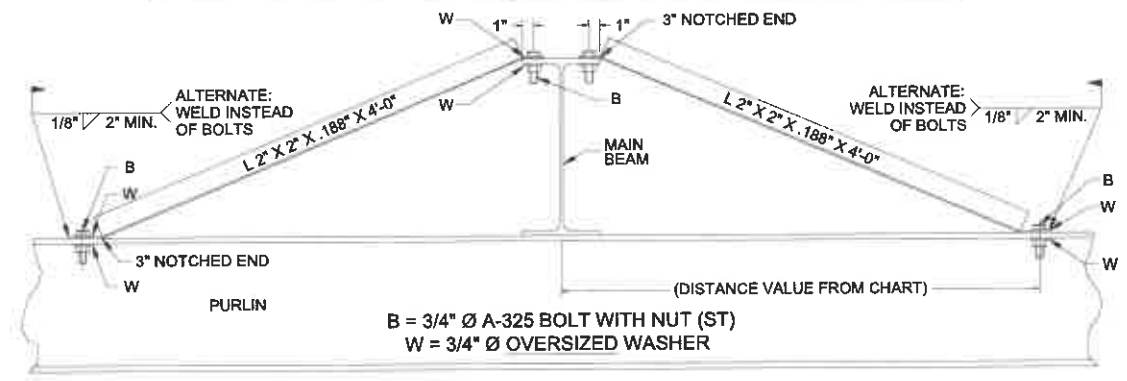


**STRUCTURAL CONNECTIONS
 DETAIL "SCDM"**

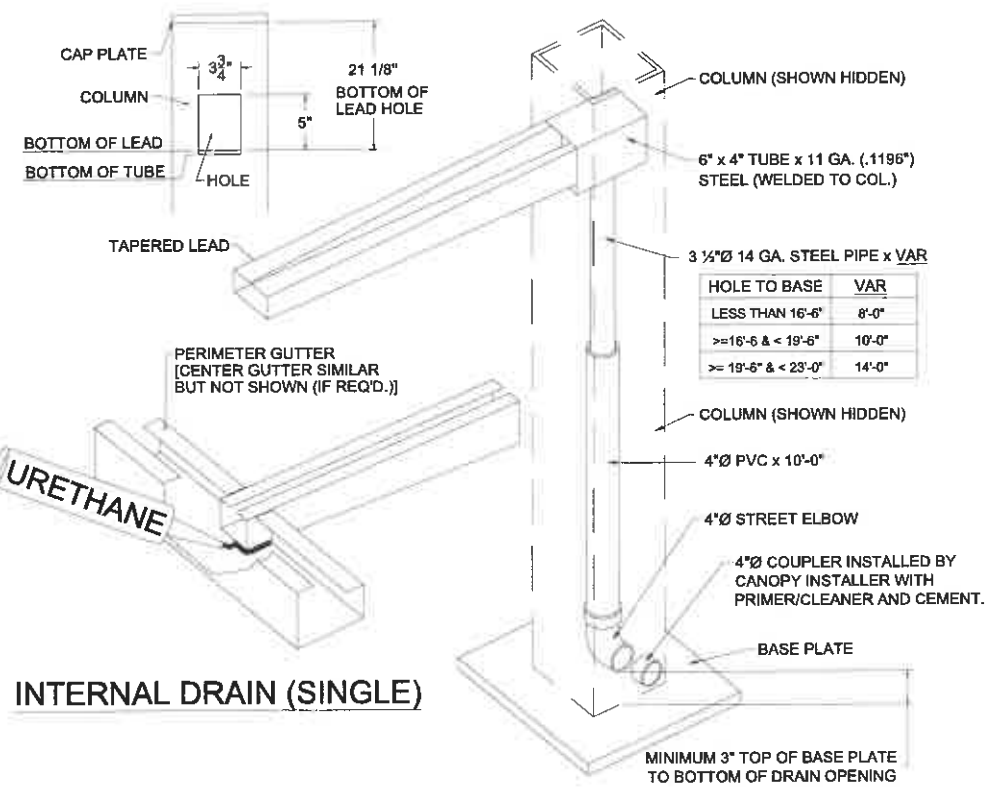


**BASE PLATE DETAIL
 BP4S10-1"**

MAIN BEAM SIZE	W8x18	W12x19	W14x22	W16x26	W16x31	W16x36	W18x50
DIMENSION "X"	46 1/2"	44 15/16"	44 15/16"	44 7/16"	44 3/8"	45 1/8"	44 1/2"



BOLTED 4'-0" SAG ANGLE DETAIL

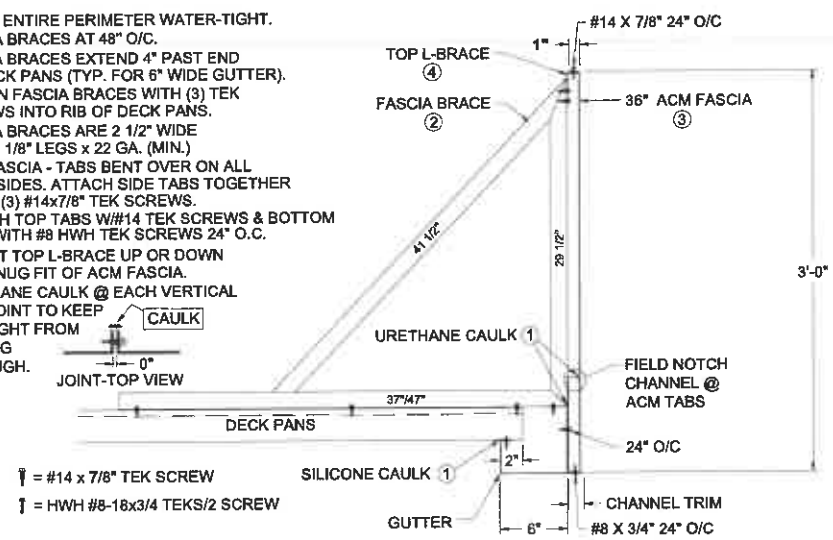


INTERNAL DRAIN (SINGLE)

HOLE TO BASE	VAR
LESS THAN 16'-6"	8'-0"
>= 16'-6" & < 19'-6"	10'-0"
>= 19'-6" & < 23'-0"	14'-0"

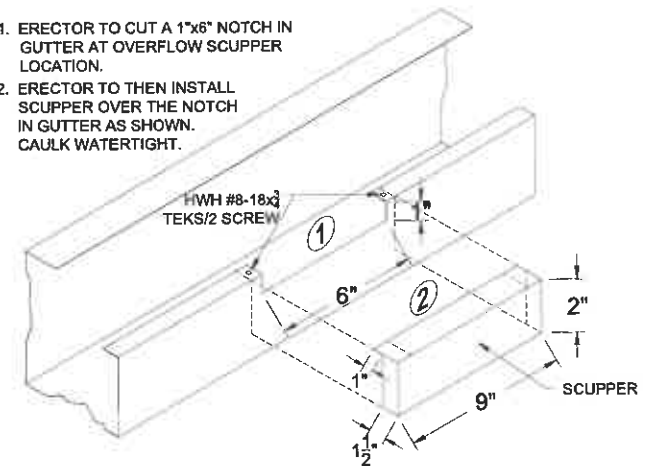
NOTES

- CAULK ENTIRE PERIMETER WATER-TIGHT.
- FASCIA BRACES AT 48" O/C.
 - FASCIA BRACES EXTEND 4" PAST END OF DECK PANS (TYP. FOR 6" WIDE GUTTER).
 - FASTEN FASCIA BRACES WITH (3) TEK SCREWS INTO RIB OF DECK PANS.
- ACM FASCIA - TABS BENT OVER ON ALL FOUR SIDES. ATTACH SIDE TABS TOGETHER USING (3) #14x7/8" TEK SCREWS. ATTACH TOP TABS W/#14 TEK SCREWS & BOTTOM TABS WITH #8 HWH TEK SCREWS 24" O.C.
- ADJUST TOP L-BRACE UP OR DOWN FOR SNUG FIT OF ACM FASCIA.
- URETHANE CAULK @ EACH VERTICAL ACM JOINT TO KEEP ANY LIGHT FROM SHINING THROUGH.

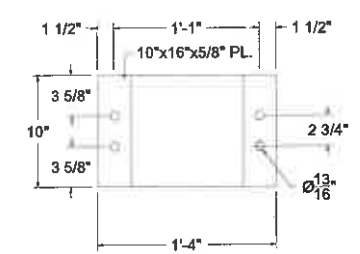


36" ACM FASCIA

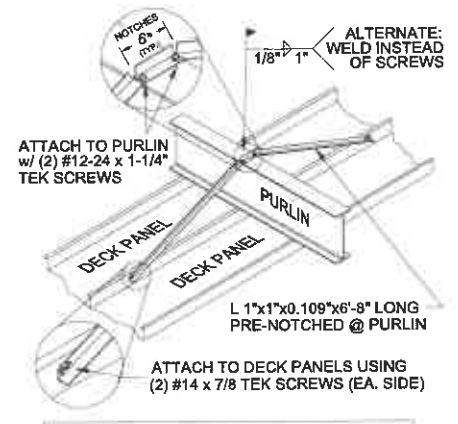
- ERECTOR TO CUT A 1"x6" NOTCH IN GUTTER AT OVERFLOW SCUPPER LOCATION.
- ERECTOR TO THEN INSTALL SCUPPER OVER THE NOTCH IN GUTTER AS SHOWN. CAULK WATERTIGHT.



OVERFLOW SCUPPER



**CAP PLATE DETAIL
 CPM10**



PURLIN ROLLOVER DETAIL

REV.	DESCRIPTION	DATE	BY

ERECTOR CONSTR STEEL JOB FILE

JOB #

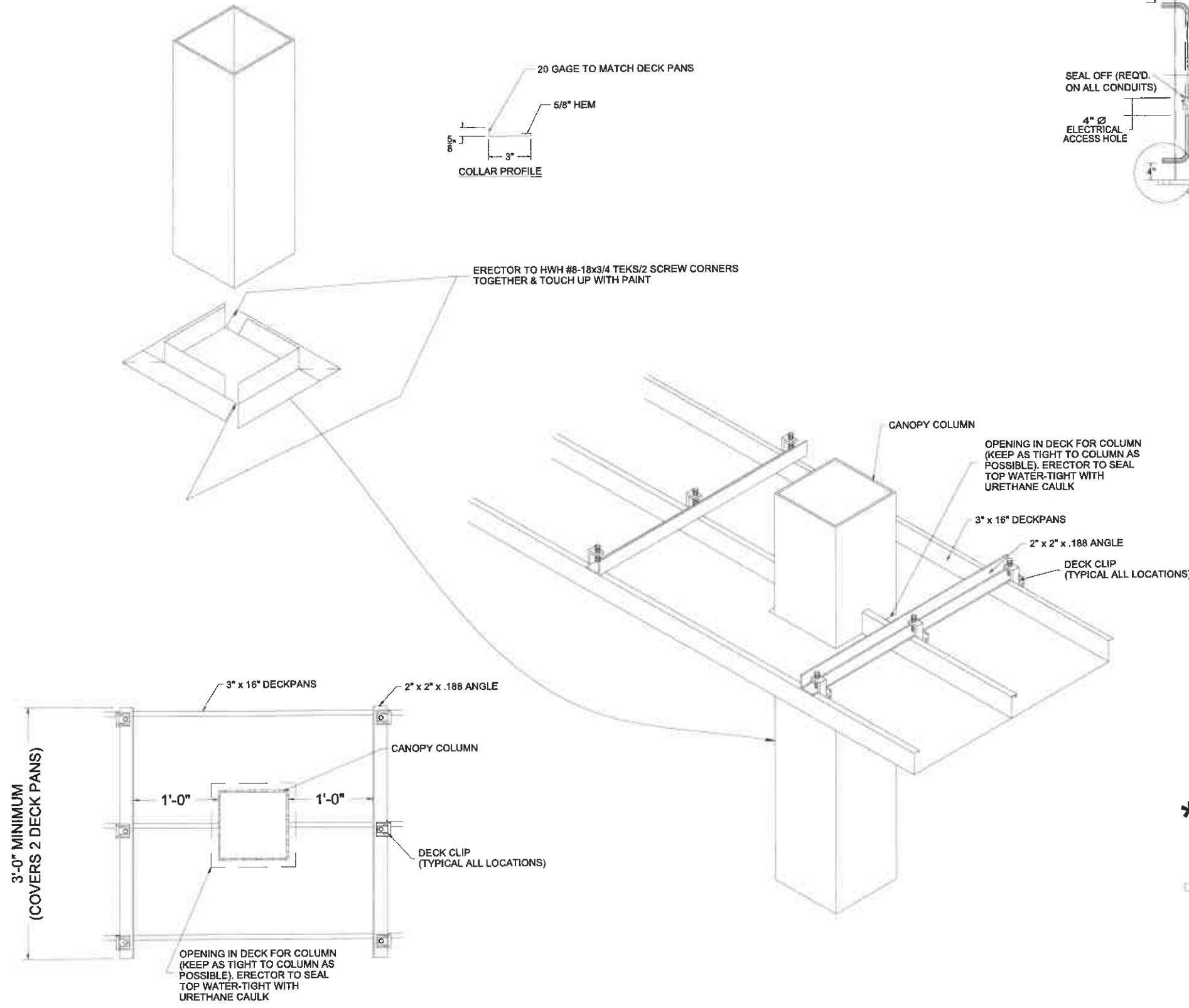
DETAILS

JOB REF. N/A

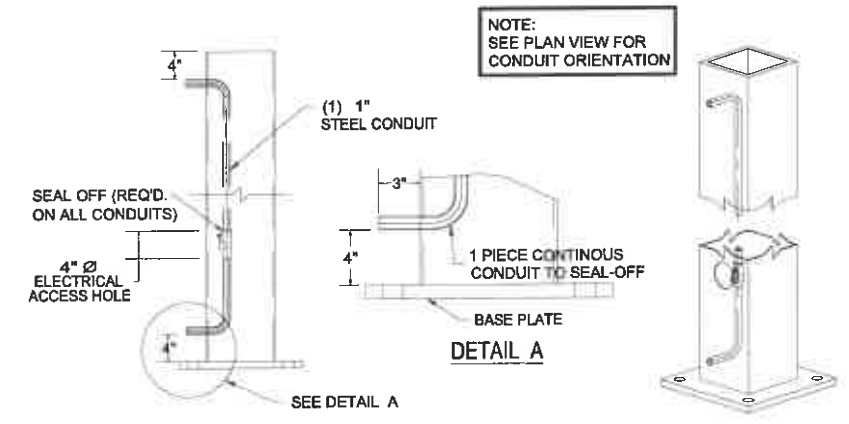
TFC

TFC CANOPY
 1107 N. TAYLOR RD.
 GARRETT, IN 46738
 PH. (260) 357-6665 FAX (260) 357-6533

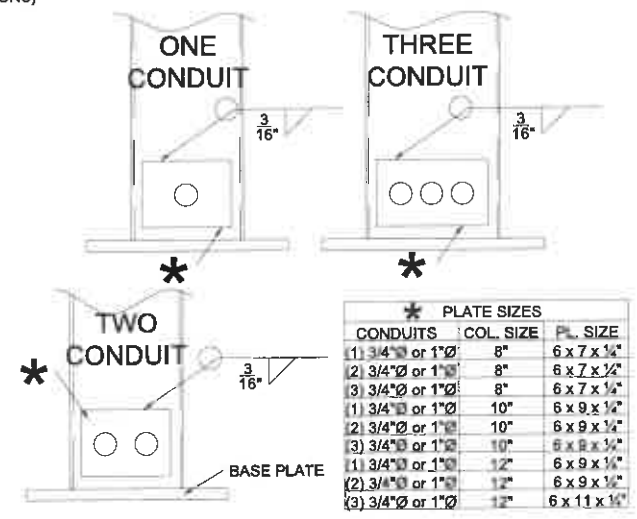
LOCATION:	Roden Park C-Store (Arco-Diesel) Monroe, Ohio
CUSTOMER:	JF Petroleum Group Dayton, OH
SCALE:	DO NOT SCALE
ENGINEER:	KED
DATE:	Rev. 12/2023
DRAWN BY:	BTD
DRAWING NO.:	2 OF 3



DECK SUPPORT AT COLUMNS



INTERNAL CONDUIT DETAIL
COLUMN # T.B.D



REINFORCEMENT DETAIL

TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-6665 FAX (260) 357-6533

LOCATION: Roden Park C-Store (Arco-Diesel)
Monroe, Ohio

CUSTOMER: JF Petroleum Group
Dayton, OH

SCALE: DO NOT SCALE ENGINEER: KED

DATE: Rev. 12/2023 DRAWN BY: BTD DRAWING NO. 3 OF 3

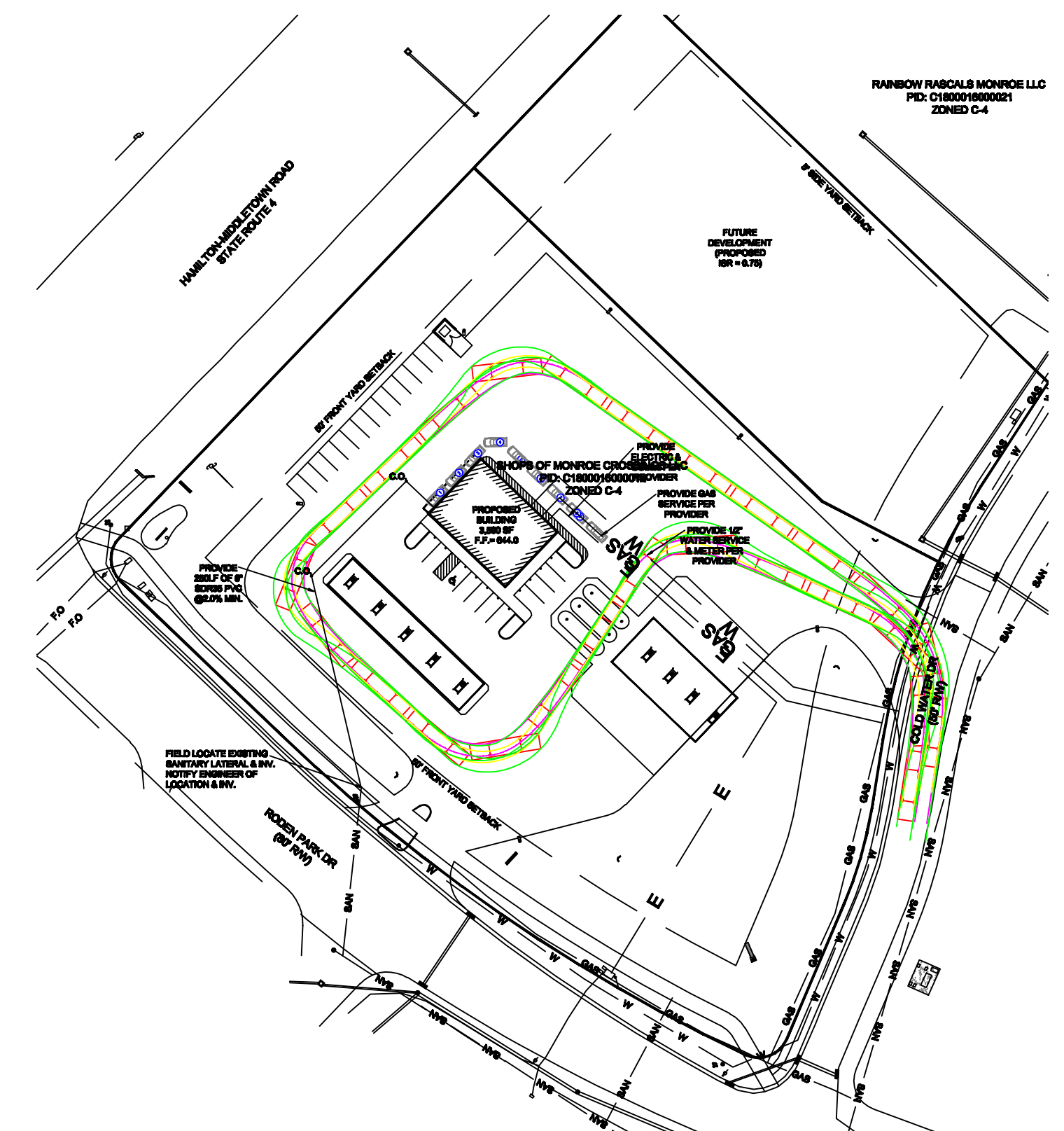
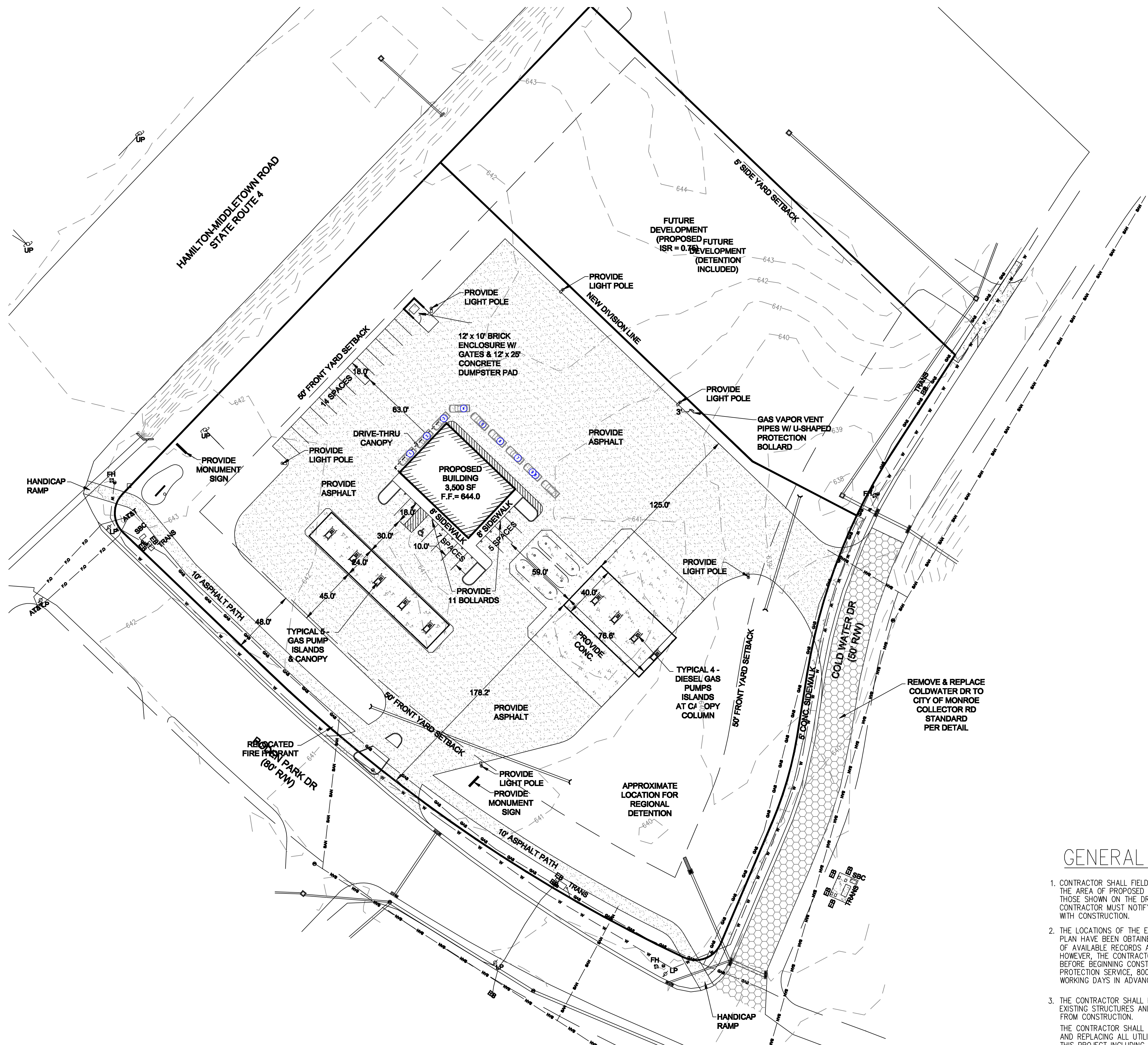
REV.	DESCRIPTION	DATE	BY

ERECTOR CONSTR STEEL JOB FILE

JOB #

DETAILS

JOB REF. N/A



GENERAL NOTES

- CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONDITIONS IN THE AREA OF PROPOSED WORK. IF CONDITIONS OTHER THAN THOSE SHOWN ON THE DRAWINGS ARE ENCOUNTERED, THE CONTRACTOR MUST NOTIFY THE ENGINEER BEFORE PROCEEDING WITH CONSTRUCTION.
- THE LOCATIONS OF THE EXISTING UTILITIES SHOWN ON THE PLAN HAVE BEEN OBTAINED BY FIELD CHECKS AND SEARCHES OF AVAILABLE RECORDS AND ARE BELIEVED TO BE CORRECT. HOWEVER, THE CONTRACTOR MUST VERIFY EXACT LOCATIONS BEFORE BEGINNING CONSTRUCTION. CONTACT THE UTILITIES PROTECTION SERVICE, 800-362-2764, AT LEAST TWO (2) WORKING DAYS IN ADVANCE OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO EXISTING STRUCTURES AND UTILITIES DURING OR RESULTING FROM CONSTRUCTION.
THE CONTRACTOR SHALL BE RESPONSIBLE FOR RELOCATING AND REPLACING ALL UTILITIES IGNORED TO COMPLETE THIS PROJECT INCLUDING ALL OVERHEAD AND UNDERGROUND UTILITIES.
THE CONTRACTOR IS RESPONSIBLE FOR ALL PERMITS, INSPECTIONS, ETC. COORDINATION REQUIRED BY THE APPLICABLE UTILITY. THE CONTRACTOR SHALL MAKE ALL RESTORATION, REPLACEMENT, AND REPAIR IN ACCORD WITH THE APPLICABLE REQUIREMENTS OF THE UTILITY.
- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE STATE OF OHIO CONSTRUCTION AND MATERIAL SPECIFICATIONS

ZONING

CITY OF MONROE
BUTLER COUNTY, OHIO
ZONE (C-4): GATEWAY COMMERCIAL

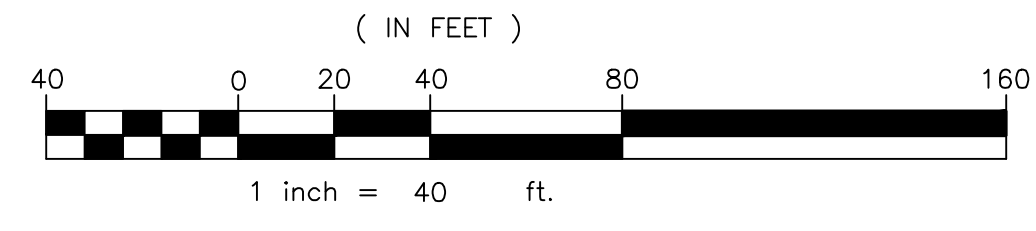
SETBACKS:
FRONT YARD=50'
SIDE YARD=5'
REAR YARD=5'

PARKING REQUIREMENTS

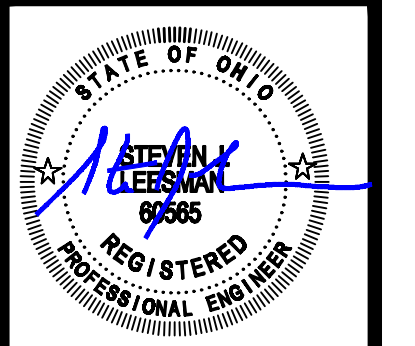
Retail Fuel Sales: One space per 300 square feet of indoor sales area plus one space per fuel pump or service bay (service bay may not be counted as a parking space)

REQUIRED PARKING SPACES: 9 pumps = 9 Spaces
REQUIRED PARKING SPACES: 4661 sf/300 = 15.5 spaces

TOTAL REQUIRED PARKING SPACES: 25 spaces
TOTAL PROVIDED PARKING SPACES: 25 Spaces

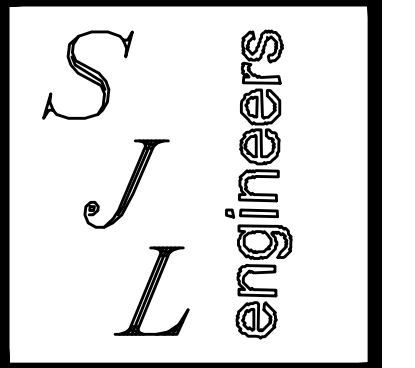


NO.	DESCRIPTION	BY	DATE
1	REVISIONS AND/OR ISSUES		



RODEN PARK C-STORE
RODEN PARK DR, MONROE OH 45044

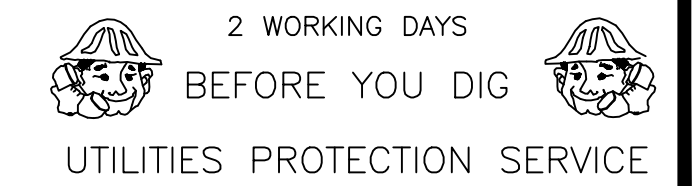
LEESMAN ENGINEERING & ASSOC.
ENGINEERING, SURVEYING, PLANNING
2720 TOPIC HILLS, CINCINNATI, OHIO 45248 513-947-0480

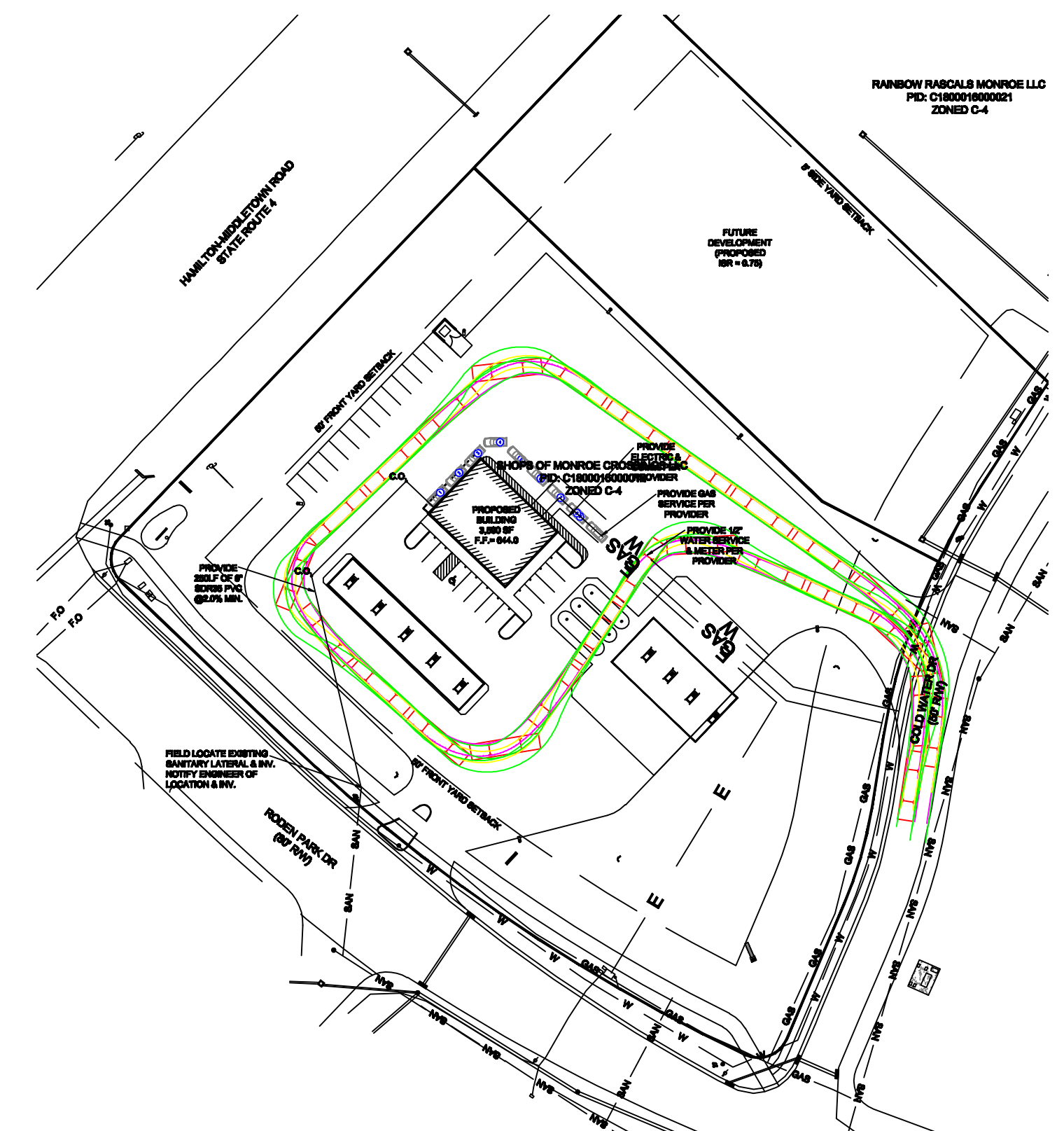
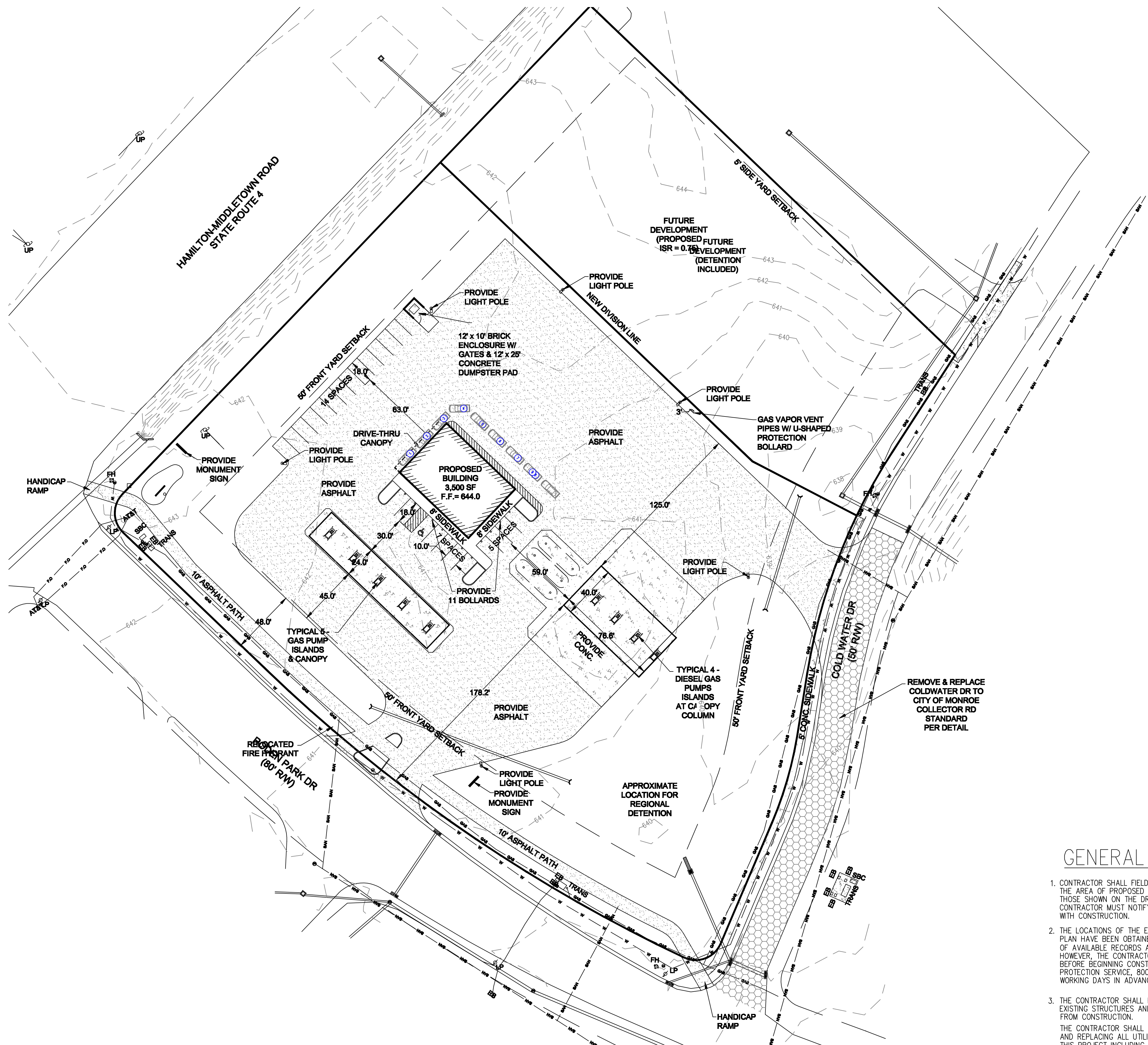


DRAWING TITLE

SITE PLAN	
PROJ. NO. C-STORE	DATE 12-7-23
DRAWN JER	CHECKED SJL

DRAWING NO.
C2





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ZONING

CITY OF MONROE
BUTLER COUNTY, OHIO
ZONE (C-4): GATEWAY COMMERCIAL

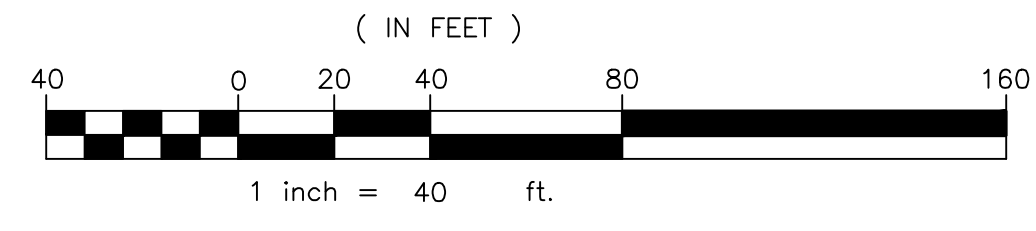
SETBACKS:
FRONT YARD=50'
SIDE YARD=5'
REAR YARD=5'

PARKING REQUIREMENTS

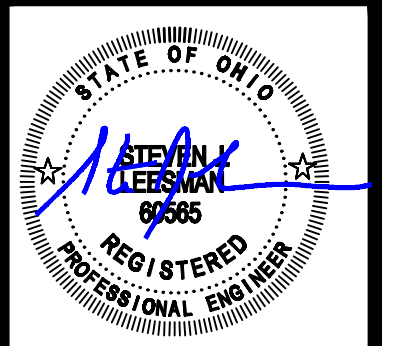
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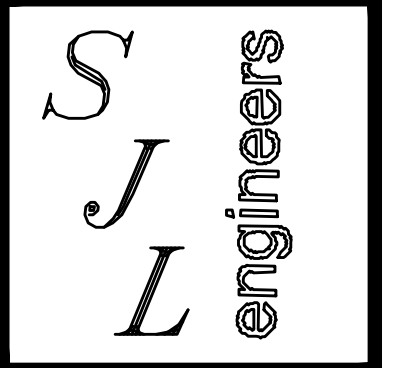


NO.	DESCRIPTION	BY	DATE
1			



RODEN PARK C-STORE
RODEN PARK DR, MONROE OH 45044

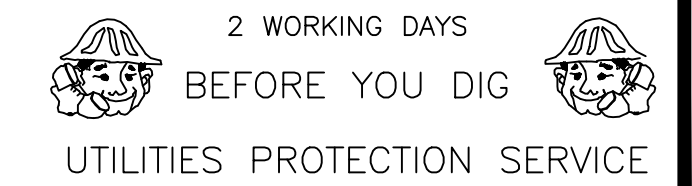
LEESMAN ENGINEERING & ASSOC.
ENGINEERING, SURVEYING, PLANNING
2720 TOPIC HILLS, CINCINNATI, OHIO 45248 513-947-0480



DRAWING TITLE

SITE PLAN	
PROJ. NO. C-STORE	DATE 12-7-23
DRAWN JER	CHECKED SUL

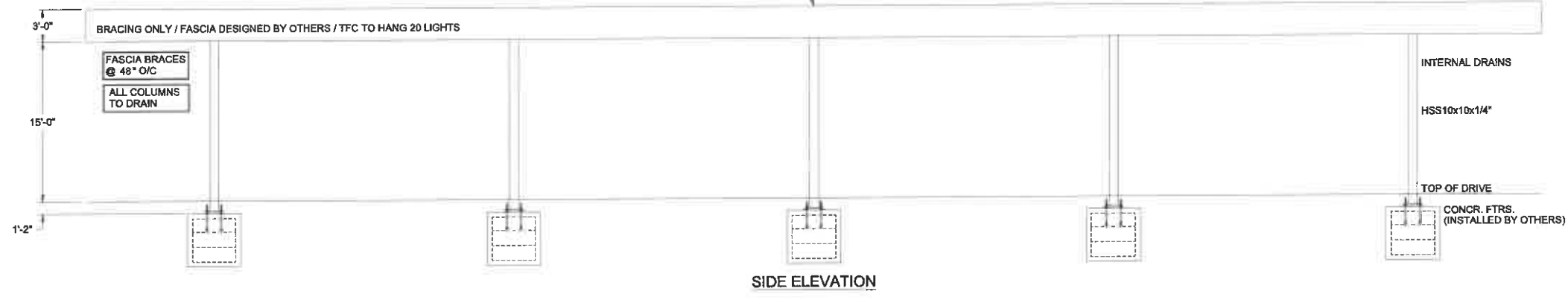
DRAWING NO.
C2



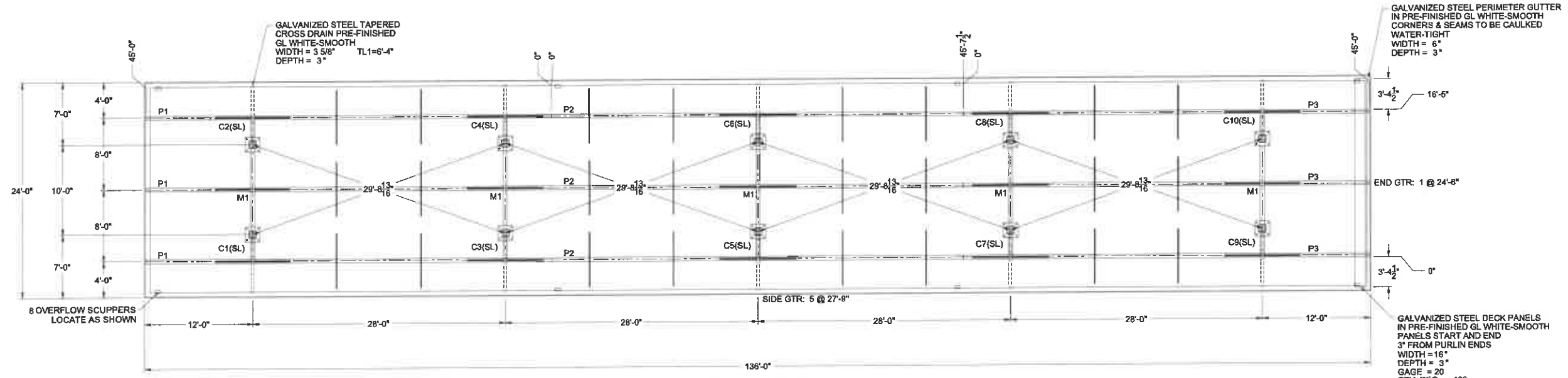
- 1'-2" PURLIN HEIGHT
- 1'-2" FOOTER SUBSET
- 15'-0" CLEARANCE HEIGHT
- + 0'-8" DECK & GUTTER DEPTH
- 17'-10" FOOTER TO MAINBEAM HEIGHT
- 0'-3" TOP & BOTTOM PLATE THICKNESS
- 17'-7" COLUMN CUT LENGTH

FASCIA INSTALLATION:
SEE PANEL LAYOUT IN MATERIAL LIST.
SEE GRAPHICS LAYOUT IF APPLICABLE.

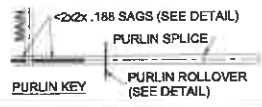
ERECTOR NOTE: ALL BOLTS ARE 3/4"Ø



SIDE ELEVATION



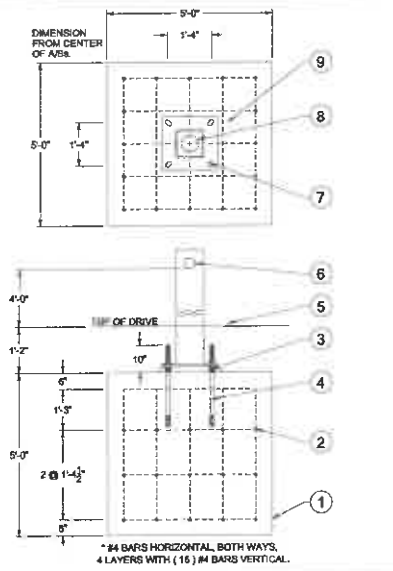
CANOPY PLAN VIEW
PURLINS ARE ("P" MARKS)
W14X22 -U.N.O.
MAIN BEAMS ARE ("M" MARKS)
W14X22 -U.N.O.



FOOTER NOTES

1. CONCRETE BASE BY GENERAL CONTRACTOR.
2. SEE FOOTER DETAIL FOR REQUIRED REBAR SPACING, REBAR $F_y = 90$ ksi.
3. BASE PLATE TO BEAR ON LEVELING NUTS AND WASHERS TO CREATE A MINIMUM 1"Ø GROUT POCKET. GENERAL CONTRACTOR MUST FILL UNDER THE BASE PLATE WITH A NON-SHRINK GROUT THAT MEETS THE PERFORMANCE REQUIREMENTS OF ASTM C 1107 AND CRD-C 621 SPECIFICATIONS FOR NON-SHRINK GROUT. MIXING AND PLACING OF GROUT SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
4. 1 1/4"Ø x 30" LG. F1554 GRADE 55 ANCHOR BOLTS W/ HEAVY HEX NUT AND 10' THREADED PROJECTION WITH DOUBLE NUTS FOR PLUMBING AND LEVELING.
5. ALLOW A 36" SQUARE OPENING IN DRIVE AT COLUMNS FOR INSTALLATION OF ANCHOR BOLT NUTS. OPENING TO BE FILLED WITH CONCRETE AFTER ERECTION.
6. ELECTRICAL ACCESS OPENING (4"Ø HOLE) WITH 5/8" COVER PLATE LOCATED APPROXIMATELY 4" ABOVE TOP OF DRIVE.
7. BASE PLATES ARE 20"x20" x 1" THK. GR. 50 STL. W/ (4) - 1 1/2"Ø BOLT HOLES ON 16" x 16" CENTERS.
8. BASE PLATE HAS A 3/8" HOLE FOR ELECTRICAL ACCESS.
9. UNLESS OTHERWISE NOTED, TOPS OF FOOTERS MUST BE HELD LEVEL AND ON THE SAME PLANE.
10. CONCRETE FOR FOOTERS SHALL ACHIEVE A MINIMUM COMPRESSION OF 4000 PSI.
11. FOOTING DESIGN IS BASED ON PRESUMPTIVE LOAD BEARING VALUES PROVIDED IN THE INTERNATIONAL BUILDING CODE, TABLE 1606.2. AN ASSUMED ALLOWABLE BEARING PRESSURE OF 6000 PSF AND A LATERAL BEARING PRESSURE OF 150 PSF/FT SHALL BE VERIFIED IN THE FIELD. IT IS RECOMMENDED THAT FOOTINGS BEAR ON STABLE ORIGINAL, UNDISTURBED SUBGRADE OR SELECT CONTROLLED FILL (EXTENDING TO STABLE ORIGINAL, UNDISTURBED SUBGRADE). IF FIELD CONDITIONS AND TESTING INDICATE LESS CAPACITY OR DISTURBED SOIL, THE DETAIL SHOWN SHALL NOT BE USED AND TFC SHALL BE CONTACTED IMMEDIATELY TO REDESIGN THE FOOTINGS.

FOOTER DETAILS



TFC

TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-6665 FAX (260) 357-6533

LOCATION:	Roden Park C-Store Monroe, Ohio
CUSTOMER:	JF Petroleum Group Dayton, OH
SCALE:	DO NOT SCALE
DATE:	Rev. 12/2023
ENGINEER:	KED
DRAWN BY:	BTD
DRAWING NO.:	1 OF 3

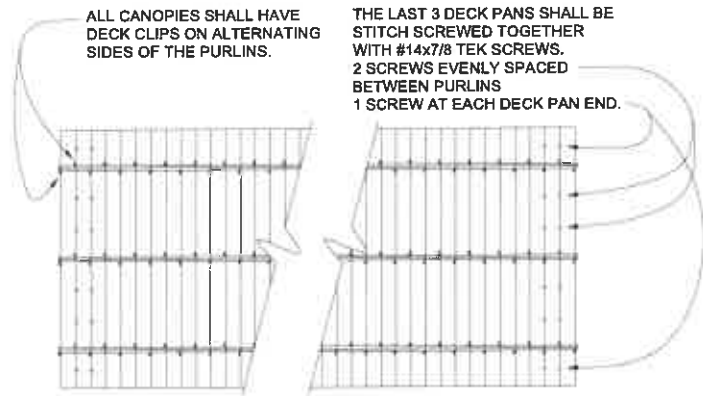
REV.	DESCRIPTION	DATE	BY

ERECTOR
 CONSTR
 STEEL
 JOB FILE

JOB #

PLAN VIEW & ELEVATIONS

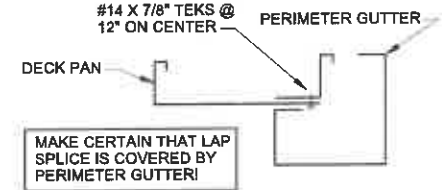
JOB REF. N/A



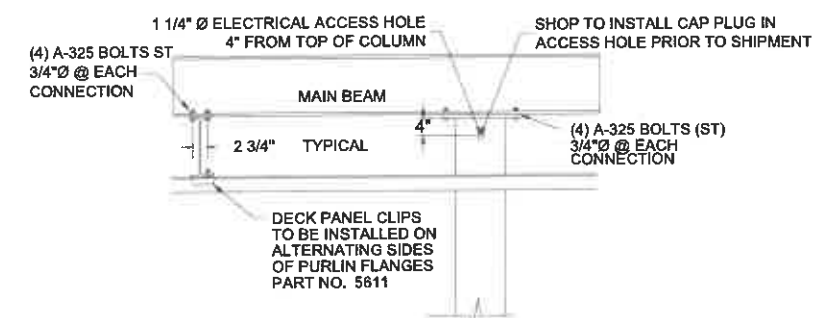
DECK ATTACHMENT DETAIL

ALL CANOPIES SHALL HAVE DECK CLIPS ON ALTERNATING SIDES OF THE PURLINS.

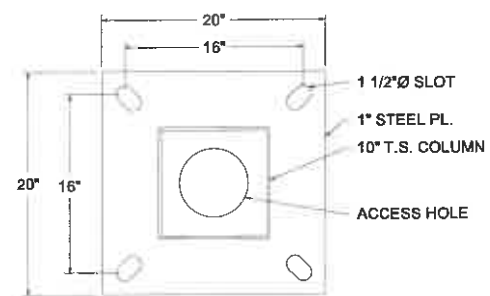
THE LAST 3 DECK PANS SHALL BE STITCH SCREWED TOGETHER WITH #14x7/8 TEK SCREWS. 2 SCREWS EVENLY SPACED BETWEEN PURLINS. 1 SCREW AT EACH DECK PAN END.



FINISH DECK PAN DETAIL

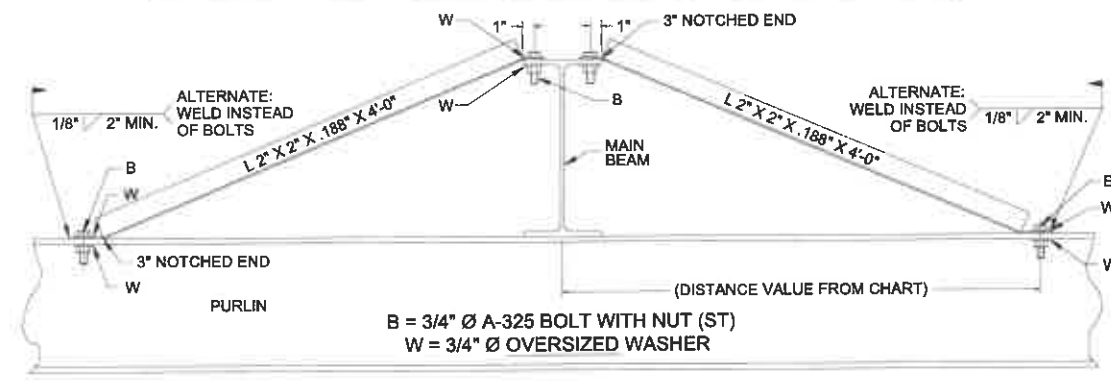


STRUCTURAL CONNECTIONS DETAIL "SCDM"

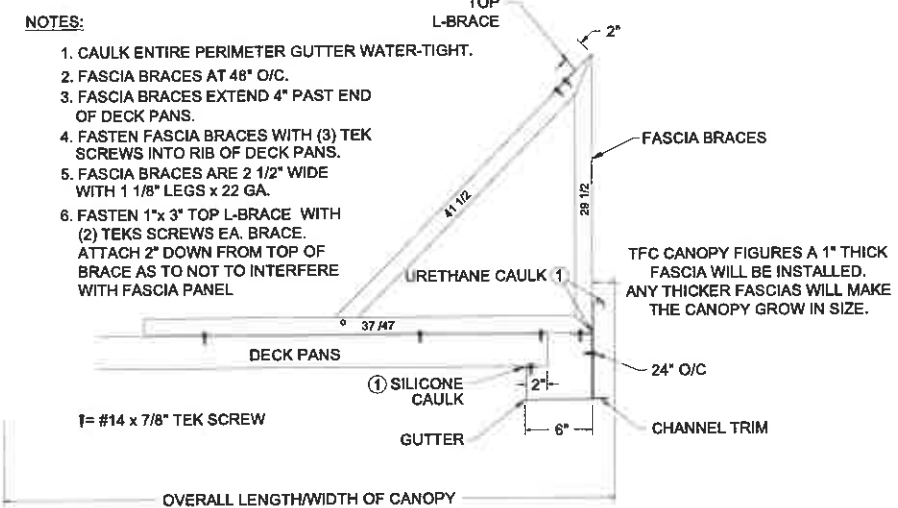


BASE PLATE DETAIL BP4S10-1"

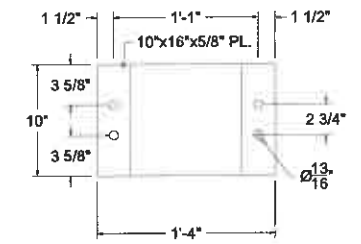
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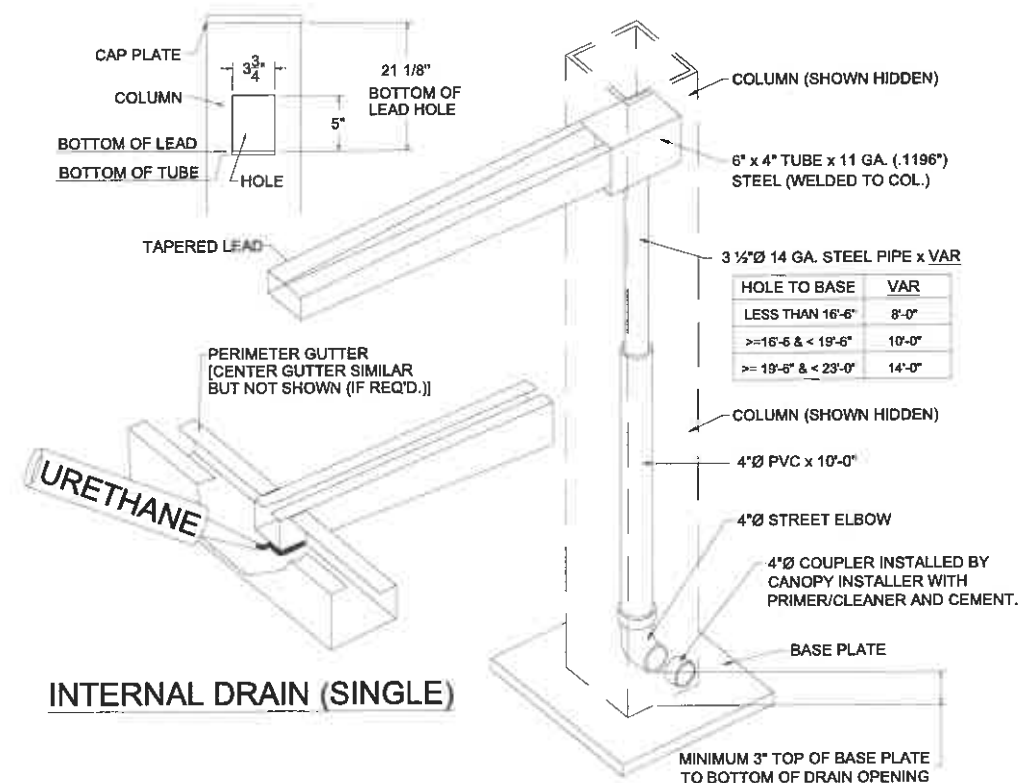
BOLTED 4'-0" SAG ANGLE DETAIL



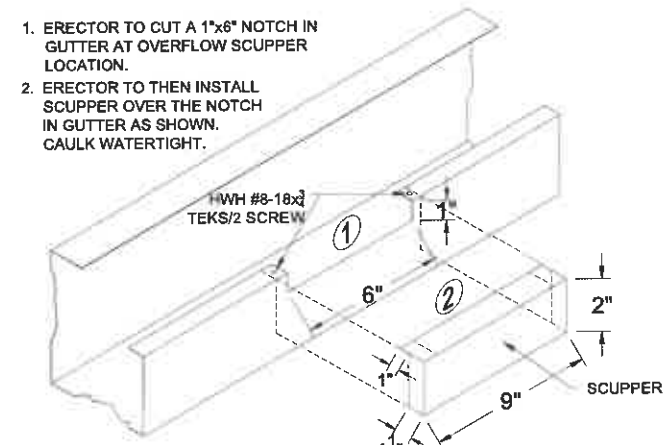
ARCO BRACING ONLY DETAIL



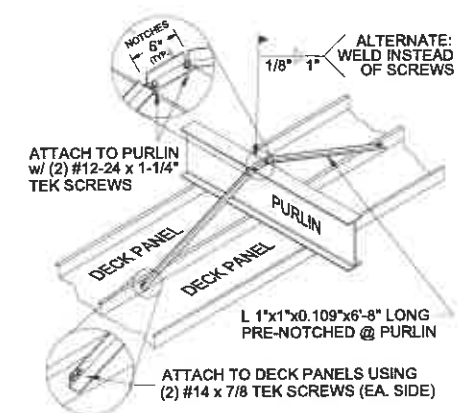
CAP PLATE DETAIL CPM10



INTERNAL DRAIN (SINGLE)



OVERFLOW SCUPPER



PURLIN ROLLOVER DETAIL

TFC

TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-6665 FAX (260) 357-6533

LOCATION: Roden Park C-Store
Monroe, Ohio

CUSTOMER: JF Petroleum Group
Dayton, OH

SCALE: DO NOT SCALE

ENGINEER: KED

DATE: Rev. 12/2023

DRAWN BY: BTD

DRAWING NO. 2 OF 3

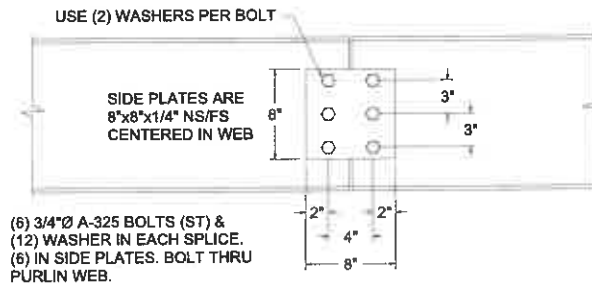
REV.	DESCRIPTION	DATE	BY

ERECTOR CONSTR STEEL JOB FILE

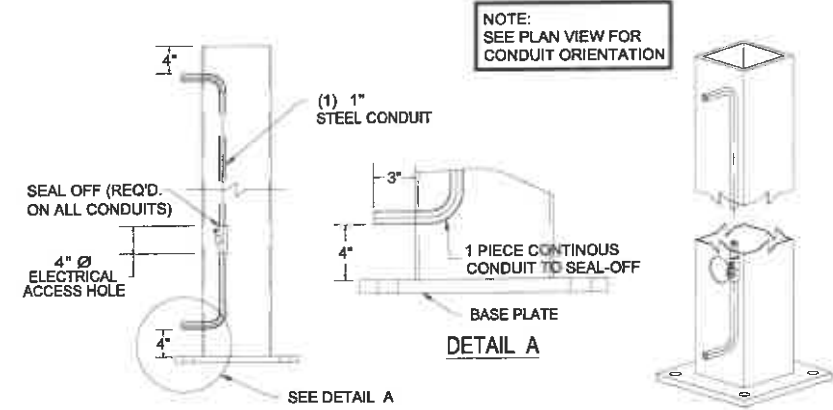
JOB #

DETAILS

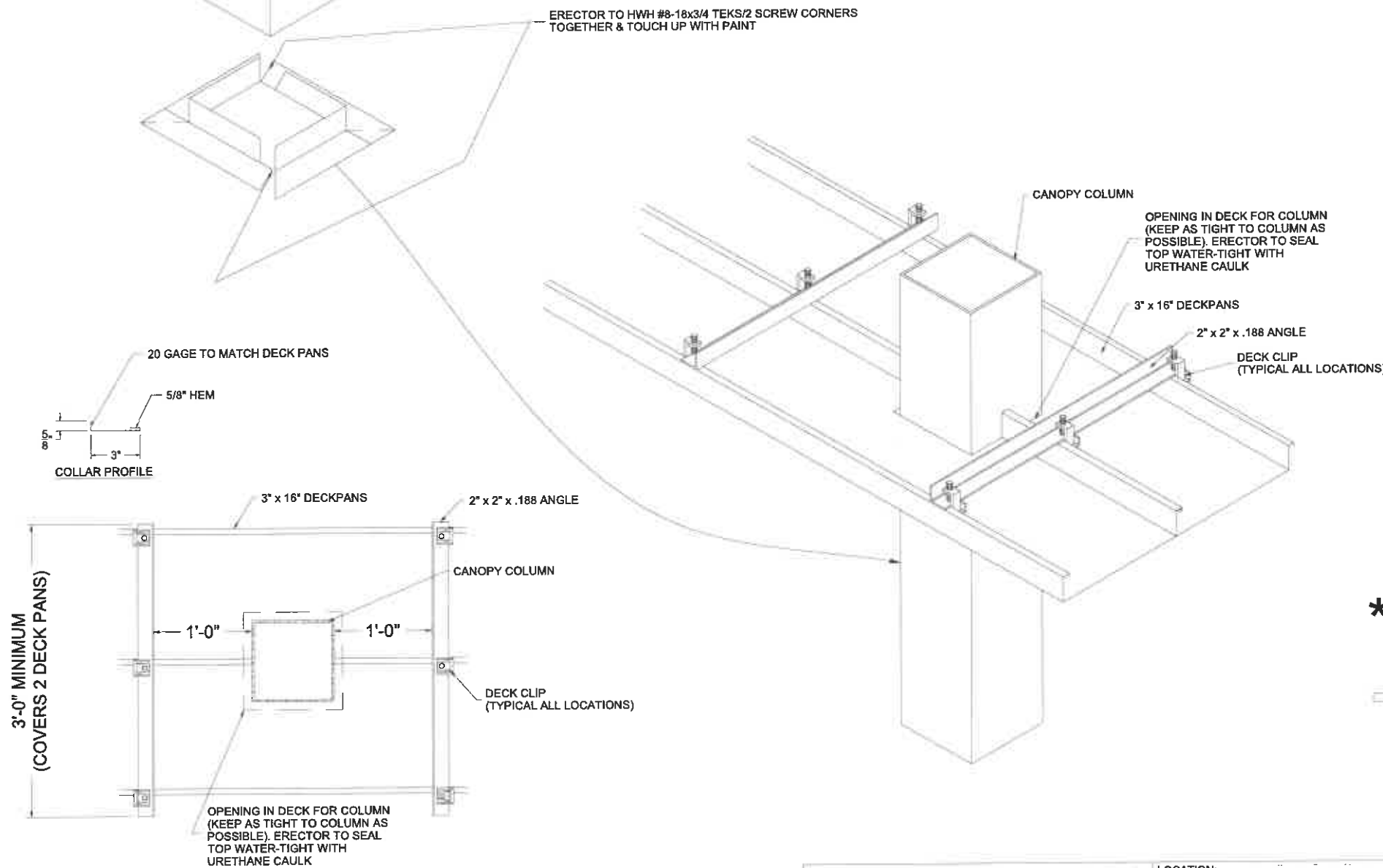
JOB REF. N/A



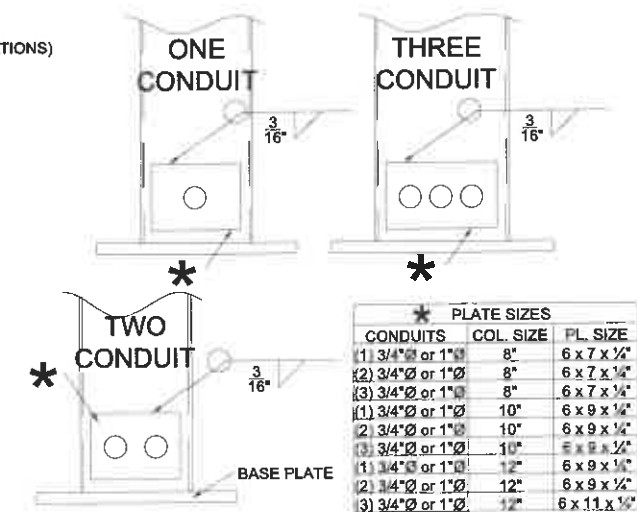
**PURLIN WEB SPLICE DETAIL
SPLB**



**INTERNAL CONDUIT DETAIL
COLUMN # T.B.D**



DECK SUPPORT AT COLUMNS



REINFORCEMENT DETAIL



TFC CANOPY
1107 N. TAYLOR RD.
GARRETT, IN 46738
PH. (260) 357-6665 FAX (260) 357-6533

LOCATION: Roden Park C-Store
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Dayton, OH

SCALE: DO NOT SCALE

ENGINEER: KED

DATE: Rev. 12/2023

DRAWN BY: BTD

DRAWING NO.
3 OF 3

REV.	DESCRIPTION	DATE	BY

ERECTOR CONSTR STEEL JOB FILE

JOB #

DETAILS

JOB REF. N/A



Application for a Variance

Monroe Development Department
 233 South Main Street, Monroe, Ohio 45050
 Phone: 513-539-7374
 planning@monroehio.org
www.monroehio.org

For Staff Use Only

Submittal Date:

Date Application Determined Complete:

Date of BZA Review:

Fee Paid: **RES/AG- \$300 COMM- \$500**

Staff Initials:

Application/Case Number:

Variance Review Information

1. The variance review procedure and review requirements are established in Section 1203.08 of the Monroe Planning and Zoning Code.
2. There are separate review criteria for an area/dimensional variance (a request to vary any numerical or dimensional requirement in the code) and for a use variance (a request to have a use approved that is not otherwise allowed in the applicable zoning district. The applicant must demonstrate a practical difficulty for an area/dimensional variance or an unnecessary hardship for a use variance.
3. Approval of a variance does not guarantee any other approvals (e.g., site plan or zoning permit reviews). The applicant shall be responsible for applying for any other necessary permits or reviews.
4. Applicants shall obtain a zoning compliance inspection within one year of a variance approval or the approval shall be voided.
5. The application fee for a variance is \$300 for a residential or agricultural use and \$500 for all other uses, due at the time the application is submitted.

Basic Project and Site Information

Project Address: 4592 ENT MONROE CROSSINGS

County: Butler Warren

Total Lot Area: 5.351 Acres

County Tax Parcel ID: C1800016000012

Project/Development Name: MONROE C-STORE

Existing Zoning District: (C-4): Gateway Commercial

Variance Type: Area/Dimensional Variance Use Variance

Applicant Information

Applicant Name: CARRA BUILDERS

Contact Person: JASON CALDWELL

Contact Address: 335 HULB DRIVE CLAYTON OHIO 45315

Contact Phone Number: 937-536-0251

Contact Fax Number:

Contact E-Mail: j.caldwell@carrabuilders.com

Property Owner Name: SHOPS OF MONROE CROSSING LLC HARRY THOMAS JR.

The applicant may attach a separate sheet of a paper with contact information for additional people, if necessary (e.g., additional owners, registered engineer, landscape architect, etc.). The same contact information shall be provided for each contact name.

Project Address or Tax Parcel ID: C1800016000012

Additional Project Information Continued

6) Will the granting of the variance request(s) be consistent with the surrounding neighborhood and will the variance request(s) negatively impact or impair the essential character of the area?

Yes No Please explain your rationale below.

The Fuel Canopy has been an allowable use elsewhere in the City of Monroe, just requires the adjusted 15' clearance and 18' total height.

7) To the best of your knowledge, is the difficulty or hardship described in this application self-imposed? Are the circumstances surrounding the request the result of the actions of the owner?

Yes No Please explain your rationale below.

Proposed part and use of this location's business model. For fueling sales requires adjusted clearance and total height of fueling canopies.

8) Identify how the proposed variance application will protect the surrounding properties from any negative impacts (e.g., noise, light, traffic, etc.)

Proposed design meets standards as well as lighting and the additional improvement of Coldwater Rd. Pavement improvement by the owner is a benefit to the City of Monroe infrastructure.

Project Address or Tax Parcel ID: C 1800 160 00012

Additional Project Information

1) Please describe, in detail, the proposed uses and/or the proposed development.

Fuel station C-STORE w/ STANDARD \pm TRUCK
Fuel CANopies
15' CLEARANCE and 18' TOTAL Height

2) Identify what variances are being sought including the applicable code sections, required dimensions, or the established standard.

ALLOWANCE OF Fuel CANopies - 2 Separated
one For TRUCK Deisel Fueling 15' CLEARANCE 18' TOTAL
Height

3) What are the characteristics of the property that prevent you from meeting the minimum requirements of the planning and zoning code?

- Lot is too narrow Lot is too small Lot is too shallow The soil or subsurface is unique to the lot
 The shape of the lot Building or lot elevation Topography Other (Describe in #4 below.)

4) Explain how items checked above prevent the reasonable use of the property under the existing requirements.

PROPOSED TRUCK Fuel CANOPY

5) Explain how the property cannot yield a reasonable return or whether there can be any beneficial use of the property without the variance.

Proposed TRUCK Fuel CANOPY Intrecut to the
operation of the Fuel station C-STORE Requiring
A Slightly Higher Clearance FOR USE.

Project Address or Tax Parcel ID: C1800016000012

Additional Project Information Continued

9) Are the variance request(s) substantial and the minimum necessary to make possible the reasonable use of the land and/or structures on the property?

Yes No Please explain your rationale below.

Proposed Use Description allowable thro Pre-Development Comments and Review of CANOPY Heights of 15' clearance and 18' TOTAL Height

10) Will the granting of the variance impact governmental services such as water, sewer, refuse pickup, safety services, etc.?

Yes No Please explain your rationale below.

11) Explain how the property owner's predicament cannot be feasibly removed through another method other than a variance.

16 Combined Use Intent For this Fueling Location and Provides an additional service to the surrounding Business infrastructure existing. DOES HOWEVER Require INCREASED 15' CLEARANCE and 18' Total Canopy Height

Project Address or Tax Parcel ID: C1800016000012

Additional Project Information Continued

12) Would the spirit and intent behind the code requirement be observed and substantial justice done by the granting of the requested variance(s)?

Yes No Please explain your rationale below.

Based on Previous allowable use elsewhere in the City of Monroeville

13) Would the granting of the variance(s) requested confer on the applicant any special privilege denied by the code regulation(s) to other lands, structures, or buildings in the same zoning district?

Yes No Please explain your rationale below.

Please feel free to add any other information or rationale related to the request in the box below or as an attachment.

Project Address or Tax Parcel ID: C-18000160 00012

Certification and Signatures

Applicant Signature - I certify that, to the best of my knowledge, the information contained in this form and within any attachments is correct and truthful. I understand that knowingly falsifying this information may be grounds for the denial or revoking of this application and any subsequent zoning permit or other review applications.

Print Name: JASON Caldwell

Signature: *Jason S. Caldwell*

Date: 12/15/2023

Property Owner Signature - If the property owner is not the named applicant on this form, the property owner shall sign the form below acknowledging that the applicant is an authorized agent of the property owner as it relates to proposed project. If the variance request affects multiple properties, a signature from each property owner is required.

Print Name: Harry Thomas Jr. Sole member

Signature: *Harry Thomas Jr.*

Date: November 22, 2023

Note: A letter signed by the property owner, authorizing the applicant to act on their behalf may be submitted in lieu of the property owner's signature on this form.

Submittal Requirement Checklist

Submittals shall include 8 copies (minimum of 11" x 17").
Additional copies may be required based on project specifics.

Digital Submittal Requirements

All document applications shall be submitted in an electronic format compatible (Adobe PDF) with the city's computer systems.

General Information

- Variance Application Form
- Application fee
- Copies of the plans and maps established below at a reasonable scale, as determined by the Development Department, to review the variance request.
- Any additional information determined to be necessary by the Development Department.

Variance Site Plan

- Date, north arrow, scale, and a legend for all symbols – the plan shall be oriented with north to the top
- All existing lot lines and parcel numbers for each lot subject to the variance request and all lots adjacent to the subject lot, including any across a street.
- A survey, stamped and signed by a surveyor or engineer licensed in Ohio, documenting the exact boundaries and dimensions of the subject lot.
- Boundaries and dimensions of all existing and proposed buildings and structures
- All existing and proposed streets, rights-of-way, and related names
- Identification of proposed variance (e.g., required setback and proposed setback, or maximum height and proposed height)

From: [Christy Mersch](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon Truck Stop Proposal
Date: Wednesday, January 3, 2024 10:33:24 PM

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Hello,

I am a resident of Monroe Crossings and am concerned with proposal of a Marathon TRUCK STOP across from Speedway on Rt 4. I am not opposed to a gas station, but a truck stop is not in the best interest of the community. I believe this is too close to residential homes and have safety concerns having young children.

Additional concerns listed below.

- The canopy height being increased is directly related to trucking (the main reason they would need to increase the height)
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

Thanks
Christy

Sent from my iPhone

From: [Danielle Johnson](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Marathon Truck stop in front of Monroe Crossings
Date: Wednesday, January 3, 2024 3:44:51 PM

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Hi Jameson,

I am an almost 19-year resident of Monroe and Monroe Crossings. I've recently been informed that there are talks to add a Marathon location at the Rt. 4 entrance to our neighborhood, across from Speedway. While I don't have any real issue with a Marathon gas station going in there, I do have some concerns, listed below:

- I have been informed that it is being proposed for the canopy height to accommodate semi trucks and to make the location into a truck stop.
- I and other residents do not agree with a truck stop located so close to residential use or to the current daycare due to the increased noise, traffic, and safety concerns.
- We already have quite a bit of semi truck traffic coming through our neighborhood. We fear this will increase greatly if approved.
- As Vice President of our HOA Board, I have personally voiced concern with the city regarding the ability for pedestrians within the neighborhood to safely cross Crossings Blvd. to utilize our parks and walking trail. The additional truck traffic will do nothing but to exacerbate this issue.

I have learned from contacts within the city that this project is set to go before the BZA in the coming days, and I would implore you to consider the negative impact of putting a truck stop in such a highly populated area, and so close to both residential homes and a childcare center.

Thank you for your time and consideration.

Dani Johnson
203 Scarlet Oak Dr.
Monroe Crossings

From: [Elizabeth Benson](#)
To: [Jameson Cole](#)
Subject: [External Sender] Objection to variance in height ordinance for Marathon property near Monroe Crossings Neighborhood
Date: Wednesday, January 3, 2024 1:34:48 PM

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Mr Cole,

I am writing this email in regards to the new Marathon gas station that will be built in the near future. I'm not objecting to the gas station but my husband and I are objecting to any variance in any height ordinance to accommodate semi truck traffic. We are a neighborhood of 800+ houses in Monroe Crossings and we do not want increased semi truck traffic. We want to see our neighborhood preserved as a neighborhood. Monroe in a whole has too much semi traffic and we don't want it in our neighborhood. Here specifically are a list of our concerns and objections:

- The canopy height being increased is directly related to accommodating semi-trucks - We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

We welcome a gas station but severely object to any variance changes for this business. Semi traffic would be a nightmare there and for our neighborhood. I also would like to remind City council members that we are a subdivision of 800+ houses and still growing. That's 800+ times 2 votes when it comes to re-election time. The citizens of Monroe Crossings are watching and will vote accordingly come election time.

Thank you,
Elizabeth and Gilbert Benson
2048 Bridgewater Lane

From: [Gary Helton](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Marathon Roden Park
Date: Wednesday, January 3, 2024 4:10:38 PM

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Here we go again. A business sees an opportunity to make a great profit in the city and in order to accomplish that they want to appeal to the board for a zoning change. The canopy height increase to accommodate semi tractor-trailers would violate the original commercial building codes so they want to change it. These restrictions are put in place for a reason and will not be negotiable. Build to the current zoning standards or pack your bags and move on.

Also, a truck stop would not be acceptable due to trailer traffic and excessive noise and safety issues in our neighborhood.

From: [Gauri Singh](#)
To: [Jameson Cole](#)
Subject: [External Sender] opposing to semi-truck near monroe crossing neighborhood
Date: Friday, January 5, 2024 3:25:55 PM

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Hi Cole,
Hope all is well with you !

Im a resident of Monroe crossing neighborhood and it came to my attention that CYMI - Marathon is purchasing / has purchased the land across from Speedway. We knew this would happen eventually but if they get to accommodate semi-trucks that will be very concerning and I strongly oppose it.

following points of concern:

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

I hope you understand our concern.

Thank you!

Gauri Singh
Resident monroe crossing

From: hhaven@cinci.rr.com
To: [Jameson Cole](#)
Subject: [External Sender] Marathon Station
Date: Thursday, January 4, 2024 5:48:00 PM

WARNING - This email originated from an EXTERNAL SOURCE. Do not click any links or open attachments unless you can confirm the sender.

Good evening Mr. Cole,

I am writing with concerns about putting a Marathon gas station/truck stop at the corner of Route 4 and Roden Park Drive. As a resident of Monroe Crossings and my home on Roden, this is very concerning for traffic flow. I could imagine with the entrance lane for Route 64 right in front of this property, the semi-truck would want to use Roden as access in and out of the gas station. The would impact the area in a more negative than positive way. I am all for new businesses and growing but I feel this would not be the best use of the property. If it was a stand alone gas station for non-semi traffic, I think it would be fine. I live and work in Monroe, (Glass Coatings and Concepts on Lawton Avenue), so I do care greatly for the community and have the best interest at heart. Please consider rejecting the building of the truck stop.

Thank you very much,

Howard Haven
5332 Roden Park Drive
Monroe, Ohio
45050

From: [James Campolongo](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Marathon Truck Stop Proposal
Date: Thursday, January 4, 2024 8:31:51 PM

*** WARNING - This email originated from an EXTERNAL SOURCE. Do not click any links or open attachments unless you can confirm the sender. ***

To whom it may concern:

I write this email in opposition of the proposed Marathon Truck Stop/Gas Station that would be built near the Monroe Crossing Neighborhood.

I have lived in this neighborhood for close to 12 years. I enjoy living here and would like to continue to reside in Monroe and in this neighborhood for several more years. However, with this new proposal I'm afraid I may be forced to look for another place to move my family to.

I have been a Police Officer for about 12 years and where I work there is a truck stop such as the one proposed in the city. Throughout the years working as an officer I have dealt with several issues with these type of places. From parking and traffic issues to the more serious drug and prostitution problems that I had to deal with on a daily basis. I have seen it first hand what a place like this can do to a neighborhood and a community. From my perspective approving such a proposal wouldn't only be wrong for the community it would be down right irresponsible to approve given the Daycare next to the location. Further more the damaging influence it could have on the apartments near by where several young children reside. Not to mention and plethora of children who reside in crossing neighborhood.

Though I am not opposed completely to having another gas station near by, I personally do not see the benefit of a truck stop at this location. Monroe has a rest stop on I75, a Speedway Truck Stop near I75, and Franklin has a Pilot Truck Stop just up the road. Putting a truck stop this far off the main interstate makes no sense. On top of having it located so close to neighborhood within this city.

I strongly encourage this proposal be denied as it holds no benefit to the members of this community.

Thank you
James

From: [Jerry Vail](#)
To: [Jameson Cole](#)
Subject: [External Sender] Proposed marathon truck stop
Date: Thursday, January 4, 2024 5:29:02 PM

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Just to get to the point and address the proposed marathon truck stop in front of the Monroe Crossing neighborhood.

I am a resident of Monroe Crossing and also a truck driver so I know exactly what you can expect at a truck stop and they are all the same and putting one in front of a neighborhood is a bad idea. Generally truck stops are not this far off the highway and definitely not in front of residential neighborhoods.

Truck stop are full of prostitution, drug trafficking, human trafficking, homelessness and trash and damage to property not to mention sketchy people. Also the traffic and strain on our tax funded dollars such as police and fire department. I for one not am interested in this in front of my neighborhood and I will definitely be looking to move if this happens.

Speedway looks like a dump out there on rt 4 and it's not really maintained that well so putting another even bigger eye sore on the other corner seems to be to be just doubling down.

Thanks
Jerry

From: [Jill Ferguson](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon at Monroe Crossings / Roden Park
Date: Wednesday, January 3, 2024 12:20:04 PM
Attachments: [image001.gif](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)
[image009.jpg](#)
[image010.png](#)

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Hi Jameson –

My name is Jill Ferguson and I live at 2036 Bridgewater Lane in Monroe Crossings. I heard that Marathon has purchased land at the intersection of Route 4 and Roden Park with the intention of building a gas station. It is my understanding that Marathon is also requesting an increase for the variance height to support semi trucks. We knew eventually there would be another gas station going in that location, but definitely do not want to attract semi-trucks so close to the neighborhood.

Here are a few reasons for my objection:

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved. My biggest concern with this is they don't obey the traffic signs.

I've seen in the past an 18-wheeler BLOW THROUGH the intersection at Roden Park and Crossings Blvd, which is already a dangerous intersection from trucks and general traffic using it as a cut-through. The driver was actually looking both ways to see if there was traffic coming, but if there was and/or he had a full load, there would have been no way for him to stop in time. It is common knowledge amongst anyone who lives in MC that drivers (tractor trailers, soccer Moms, construction traffic) routinely blow through that 4-way Stop with little to no regard to the traffic signs and/or the safety of others. That intersection is one of the two most dangerous in Monroe, second only to the intersection of 63 and Main St / Cin Day Rd.

More semi-truck traffic is definitely NOT needed on Roden Park, Crossings Blvd or Monroe Crossings neighborhood. Please let me know if there's anything else I can do to stop this variance from being approved.

Thank you,

- Jill Ferguson

signature_460607927




Jill Ferguson

Realtor/Marketing Expert, Spouses Who Sell Houses

A 9277 Centre Pointe Dr. Suite 200, West Chester, OH 45069

P [513-854-6101](tel:513-854-6101)

M [937-231-6966](tel:937-231-6966)

E jill.ferguson@kw.com

W scottandjillferguson.com



signature_366723351




Check out our agent bio video!

From: [Kara Dennett](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon/monroe Crossings
Date: Thursday, January 4, 2024 10:54:35 PM

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Hi Mr. Cole,

My name is Kara Berndt and I am a resident of Monroe Crossings. I have been informed that a marathon truck stop is wanting to be added right outside of our neighborhood. I ask that you please help this NOT to happen. This won't be good for our neighborhood or Monroe.

I think that this will add unwanted traffic and noise. I also think it is a safety issue for our children having a truck stop right outside the neighborhood and next to a daycare.

I really hope that you can stop this from happening. We love our neighborhood and I don't think that this is a step in a positive direction.

Thank you for your time.

Kara Berndt

From: [KARLA BEVERLY](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Marathon purchasing land across from Speedway
Date: Wednesday, January 3, 2024 4:18:23 PM

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Jameson,

This in reference to the land by Speedway at Rt 4 & Roden Park leading into Monroe Crossings.

The following are points of concern:

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

My husband & I live up off East Brooke Dr down towards Griffin Ln and the semi traffic noise is already atrocious, I can't imagine how much worse it will be if you allow this to happen.

Best regards,

Karla Beverly
Kjbeverly7@yahoo.com
513-208-8279

From: [Kim Kammerdiener](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Marathon truck stop - variance
Date: Thursday, January 4, 2024 5:46:04 PM

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I live in the Monroe Crossings neighborhood at 649 Babbling Brooke drive.

I am concerned about the variance proposal to accommodate semi-trucks at the proposed site. I am not opposed to a gas station being at this location. My concern is the truck traffic in the area that would then route onto Roden Park Drive and Crossings Blvd. This is a residential area that includes children playing, on bikes, walking to parks, etc.

Due to location, the trucks will probably normally enter from Rt 4.

Areas of concern:

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

Thank you for your consideration and review

Kimberly Ross

Sent from my iPhone

From: [Lisa Walters](#)
To: [Jameson Cole](#)
Subject: [External Sender] New Marathon
Date: Wednesday, January 3, 2024 4:52:15 PM

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Hello Mr Cole,

As a resident of Monroe Crossings subdivision, I am emailing you about several safety concerns related to the new Marathon that will be built on Rte 4, across from Speedway. My understanding is that they are asking for a variance on their canopy height.

Here are my concerns:

- The canopy height being increased is directly related to accommodating semi-trucks
- I don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. I fear this will increase if approved.

I respectfully ask that this variance not be granted due to the above noted issues.

Thank you,
Lisa Walters
750 E Brooke Drive

From: [Logan C](#)
To: [Jameson Cole](#); [Council](#)
Subject: [External Sender] Proposed Marathon Truck stop
Date: Thursday, January 4, 2024 5:07:26 PM

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Mr Cole,

My wife and I are residents of Monroe crossings. It has recently been brought to our attention of the plans to potentially develop a Marathon truck stop at the corner of RT-4 and Roden park dr. While I have no issues with Marathon as a whole, I do have some concerns, that are listed below.

1. I have been informed that it is being proposed for the canopy height to accommodate semi trucks and make the location into a truck stop.
2. I have serious concerns about a truck stop being located so closely to a residential area/Daycare due to increased noise, traffic and safety concerns.
3. We already have a decent amount of semi traffic through the neighborhood due to construction of new homes (even though the city has installed signs directing semi traffic elsewhere) and fear that semi traffic will drastically increase.

I understand that this project is set to go before the BZA in the coming days, and I would implore you to consider the negative impact that this truck stop could potentially have on our neighborhood, being so close to homes and a daycare.

Thank you for your time and consideration,

Logan Clark,
Monroe resident

From: [Melissa Bramkamp](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon Station Opposition
Date: Wednesday, January 3, 2024 8:03:23 PM

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I am strongly *opposed* to semi-truck traffic near my neighborhood, Monroe Crossings. I understand Marathon has purchased the land across from Speedway at Rt 4 and Roden Park Dr. and have requested a variance to the height ordinance to accommodate semi-trucks.

My points of concern:

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

Thank you for your time,
Melissa Bramkamp
115 Songbird Ct
Monroe Crossings

Sent from my iPhone

From: [Melissa Jones](#)
To: [Jameson Cole](#)
Subject: [External Sender] Monroe Crossings
Date: Thursday, January 4, 2024 5:55:23 PM

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Hello Jameson,

I am a 13.5 year resident of MC and heard about the proposed Marathon station and truck stop.

We are opposed to this for the following reasons-

-the canopy height being increased is directly related to accommodating semi-trucks - We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.

- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.

- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase

-i'm concerned about potentially increasing traffic and noise, as well as safety concerns, as well.

We live on the side of crossings right near speedway and already the gravel and mulch company is causing additional noise- we don't need more (for us and for resale value).

Sincerely

Melissa Jones

From: [Melissa Kidd](#)
To: [Jameson Cole](#)
Subject: [External Sender] Gas Station Concern
Date: Wednesday, January 3, 2024 9:57:56 PM

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Good Evening,

Six years ago my family and I moved into Monroe Crossings to raise our children. While I understand development is necessary, I have a few concerns about the nature of this Marathon going in at the back side of our neighborhood. If they want to increase the variance height, I worry that would appeal to semi trucks, which being located very closely to a daycare and our homes opens up a world of concerns for our children. With increased traffic I worry about children out playing and riding bikes, along with increased noise at all hours. I am not against a gas station, but rather the concern is a small neighborhood community trying to support large semi trucks without compromising the safety for our children.

Thank you,
Melissa Kidd

From: [Michelle Layman](#)
To: [Jameson Cole](#); [William Layman](#)
Subject: [External Sender] Proposed Variance - BZA
Date: Wednesday, January 3, 2024 9:42:05 AM

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Good morning,

As a resident of the Monroe Crossings community I feel it is necessary to voice concerns with the proposed Marathon gas station variance request. I understand the reason for wanting an increased canopy height is directly related to being able to have diesel fueling stations for tractor trailers. I feel this will directly impact my community with increased noise, traffic, and safety concerns. The property adjacent to this is a daycare center. I can't imagine they would be happy about loud semi trucks fueling up all day either. I am aware other gas stations have been permitted this type of variance. The difference is that they are not in close proximity to a residential neighborhood. We already struggle with tractor trailer traffic driving through our neighborhood (Roden and Crossings) and feel this will increase in volume if approved. Thank you for your time and consideration to this matter.

Sincerely,

Michelle Layman

From: [Pat Betts](#)
To: [Jameson Cole](#)
Cc: [Council](#); [AAADale](#)
Subject: [External Sender] Marathon Location Rt 4
Date: Friday, January 5, 2024 12:32:36 AM

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Dear Mr. Cole,

I am a resident of Monroe Crossings. I've recently been informed that there are talks to add a Marathon location at the Rt. 4 entrance to our neighborhood, across from the existing Speedway location. While I don't have any real issue with a Marathon gas station going in there, I do have some concerns, listed below:

- I have been informed that it is being proposed for the canopy height to accommodate semi trucks and to make the location into a Truck Stop.
- I have serious concerns about a truck stop being located so close to residential use and the current daycare. I find it irresponsible, since we have an ongoing nationwide human trafficking crisis.
- Truck stops are full of prostitution, drug trafficking, littering, and damage to property (personal and city). I have seen homeless people living at the truck stops.
- The increased noise, traffic, and safety concerns.
- We already have quite a bit of semi truck traffic coming through our neighborhood. This will increase the truck traffic greatly if approved, when our neighborhood streets are already used too frequently as a cut-through.

I understand that this project is set to go before the BZA in the coming days, and I would implore you to consider the negative impact of putting a truck stop in such a highly populated area, and so close to both residential homes and a childcare center. You will find the majority of Monroe Crossings does not agree with a truck stop being placed at said location.

Thank you for your time and consideration.

Patricia Betts
doylebetts@gmail.com

Dale Betts
dalejbetts@gmail.com

From: [Becky Rosenbalm](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon Gas Station Variance
Date: Thursday, January 4, 2024 8:42:39 AM

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Hello,

I'm writing to you today as a concerned citizen regarding the proposed Marathon gas station variance request. As a local resident who resides near where this property is located, I have some concerns. The variance request, to my understanding, is for an increased canopy height for the filling station to allow tractor trailers. There is already an increased amount of semi truck traffic in the City of Monroe. These trucks already frequently travel through city neighborhoods where they are not allowed, but are just "following their GPS". I feel if this variance is allowed, this truck traffic will increase in the Monroe Crossing neighborhood which also has a daycare nearby. I'm sure with the increased truck traffic at that location, this would cause a lot of extra back-ups in and out of the neighborhood on Roden Park Dr., along with a significant amount of noise from these trucks, and them traveling through the neighborhood where they are prohibited.

Thank you for your consideration,

Rebecca Rosenbalm

From: [Tim Carpenter](#)
To: [Council](#); [Jameson Cole](#)
Subject: [External Sender] Proposed Marathon Fueling Station
Date: Thursday, January 4, 2024 8:30:10 PM

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Good evening Council and Mr. Cole,

It has been brought to my attention that there is a proposal for a new Marathon fueling station to be constructed at the intersection of Rodan Park and SR-63. Typically I do not oppose new business in our community but the details of this new fuel station has raised my concern. It would be reckless and irresponsible to allow a fueling station for tractor trailers to be constructed near residential neighborhoods, childcare centers, and near a community park. The increase in crime, vandalism, wear-and-tear on the city infrastructure, and the increased demand on our police and fire will create a burden on the entire community.

Do not allow the construction of this type of refueling station to be constructed so far away from the interstate. Please keep that traffic out of our city and as close to the interstate as possible.

Thank you,

Tim Carpenter
Board Member
Board of Education
Monroe Local School District

T: +1 513 310 2249
E: tcarpenter@monroelocalschools.com

Motivating, Leading, and Succeeding

From: [Abby Hinson](#)
To: [Jameson Cole](#)
Cc: [Council](#)
Subject: [External Sender] Proposed Truck Stop on Route 4
Date: Thursday, January 4, 2024 8:06:32 PM

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Hello Jameson,

I am reaching out to express concern over the proposed Marathon Truck Stop on Route 4 in the entrance leading to the Monroe Crossings neighborhood.

A truck stop would greatly disrupt living in Monroe Crossing by causing an increase in semi traffic and noise through our residential neighborhood. I also have safety concerns about the proximity to our local daycare.

We have so much non-residential zoning in Monroe - must we build this truck stop in one of our most densely-populated residential areas?

I love living in Monroe Crossings and hope to be here for many years to come. Thank you for all you do for the Monroe!

Abby Hinson

Sent from my iPhone

From: [Amy Dietz](#)
To: [Jameson Cole](#)
Subject: [External Sender] Monroe Crossings
Date: Wednesday, January 3, 2024 7:08:52 PM

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I have heard rumors of a Marathon truck stop potentially going off of Roden Park at Monroe Crossings. I have great concerns about this being located so close to a large residential area/neighborhood. We already have issues with trucks cutting through the neighborhood to get to 63. This will only increase this huge safety concern. It is also putting a truck stop next to a daycare which is not a good idea.

Please consider denying this request for zoning. Our neighborhood deserves better.

Amy Dietz
705 Scarlet Ct
Sent from my iPhone

From: [Tony Dirr](#)
To: [Jameson Cole](#); [Council](#)
Subject: [External Sender] truck stop proposal
Date: Thursday, January 4, 2024 5:17:09 PM

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Hello,

I am a resident of the Monroe Crossings neighborhood and am currently looking out of my kitchen window at the proposed site for the Marathon truck stop. I wanted to express my concerns regarding such an addition.

Specifically, I would like to point out that we already have a high amount of traffic using our neighborhood as a cut through that I would like to be remedied and I think that this would only add to the problem. Routinely, I see cars driving down Crossings Rd. @ at least 50 mph. Adding even more semi traffic to that road would not be a positive thing.

I don't have any particular issue with a gas station being put into that commercial space as there is already a speedway at the intersection, but a truck stop brings in more people, more non-locals, and a higher risk of unsavory activities including prostitution, trafficking, etc.

A day care center immediately next door to the site is also troubling.

Please consider these concerns when weighing whether or not to approve such an addition to our residential space.

Anthony Dirr
543 Babbling Brooke Dr.
Monroe, OH 45050

From: [candy_randolph](#)
To: [Jameson.Cole](#)
Subject: [External Sender] Monroe Crossings
Date: Wednesday, January 3, 2024 2:27:46 PM

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Dear Mr. Cole,

I live in Monroe Crossings for 18 years now and I am greatly opposed to semi truck traffic going through.

Please see the following points of concern:

- The canopy height being increased is directly related to accommodating semi-trucks - We don't want this to be close to residential use or the current daycare due to potentially increasing traffic and noise, as well as safety concerns.

Please take into consideration not allowing this variance to be approved.

Thanks
Candyce Randolph

From: [CARLA CLARK](#)
To: [Jameson Cole](#)
Subject: [External Sender] Marathon gas station
Date: Wednesday, January 3, 2024 12:57:20 PM

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Mr. Cole

I understand Marathon is purchasing the land across from Speedway near Monroe Crossings

I have heard they are asking for a variance that would allow for a higher canopy to accommodate semi-trucks. Please consider denying this request.

- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

Thank you for your consideration.

Carla Clark
Monroe resident



Chris Ritchie <mommymouse1@cinci.rr.com>

To  Jameson Cole

DONE

 Reply  Reply All  Forward 

Wed 1/3/2024 1:20 PM

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The following is a list of why we don't want semi trucks so close to our homes and the daycare.

- The canopy height being increased is directly related to accommodating semi-trucks
- We don't want this to be close to residential use or the current daycare due to the increased noise, traffic, and safety concerns.
- Other gas stations have been approved in the past, but they are not directly adjacent to residential homes.
- We already have tractor trailer traffic coming through our neighborhood. We fear this will increase if approved.

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Chris Ritchie
Concerned homeowner
Sent from my iPhone